



*Please reply to:*

Contact: Sam Nicholls  
Service: Corporate Governance  
Direct line: 01784 446240  
E-mail: [s.nicholls@spelthorne.gov.uk](mailto:s.nicholls@spelthorne.gov.uk)  
Date: 5 January 2016

## Notice of meeting

### Planning Committee

**Date:** Wednesday, 13 January 2016

**Time:** **Call Over Meeting** - 6.45 pm

The Call Over meeting will deal with administrative matters for the Planning Committee meeting. Please see guidance note on reverse

**Committee meeting** – Immediately upon the conclusion of the call over Meeting

**Place:** Council Chamber, Council Offices, Knowle Green, Staines-upon-Thames

### To the member of the Planning Committee

Councillors:

R.A. Smith-Ainsley (Chairman)  
C.M. Frazer (Vice-Chairman)  
R.O. Barratt  
I.J. Beardsmore  
S.J. Burkmar

Q.R. Edgington  
A.L. Griffiths  
I.T.E. Harvey  
N. Islam  
A.T. Jones

V.J. Leighton  
A. Neale  
O. Rybinski  
R.W. Sider BEM  
H.A. Thomson

**Spelthorne Borough Council, Council Offices, Knowle Green**

**Staines-upon-Thames TW18 1XB**

[www.spelthorne.gov.uk](http://www.spelthorne.gov.uk) [customer.services@spelthorne.gov.uk](mailto:customer.services@spelthorne.gov.uk) Telephone 01784 451499

## **Call Over Meeting**

### **Guidance Note**

The Council will organise a meeting immediately prior to the Planning Committee meeting (a "Call Over") which will deal with the following administrative matters for the Committee:

- Ward councillor speaking
- Public speakers
- Declarations of interests
- Late information
- Withdrawals
- Changes of condition
- any other procedural issues which in the opinion of the Chairman ought to be dealt with in advance of the meeting.

The Call-Over will be organised by Officers who will be present. Unless there are exceptional circumstances, the meeting will be held in the same room planned for the Committee. The Chairman of the Planning Committee will preside at the Call-Over. The Call-Over will take place in public and Officers will advise the public of the proceedings at the meeting. Public speaking at the Call-Over either in answer to the Chairman's questions or otherwise will be at the sole discretion of the Chairman and his ruling on all administrative matters for the Committee will be final.

Councillors should not seek to discuss the merits of a planning application or any other material aspect of an application during the Call-Over.

## **Planning Committee meeting**

### **Start times of agenda items**

It is impossible to predict the start and finish time of any particular item on the agenda. It may happen on occasion that the Chairman will use his discretion to re-arrange the running order of the agenda, depending on the level of public interest on an item or the amount of public speaking that may need to take place. This may mean that someone arranging to arrive later in order to only hear an item towards the middle or the end of the agenda, may miss that item altogether because it has been "brought forward" by the Chairman, or because the preceding items have been dealt with more speedily than anticipated. Therefore, if you are anxious to make certain that you hear any particular item being debated by the Planning Committee, it is recommended that you arrange to attend from the start of the meeting.

### **Background Papers**

For the purposes of the Local Government (Access to Information) Act 1985, the following documents are to be regarded as standard background papers in relation to all items:

- Letters of representation from third parties
- Consultation replies from outside bodies
- Letters or statements from or on behalf of the applicant

## AGENDA

**Page nos.**

- 1. Apologies**  
To receive any apologies for non-attendance.
- 2. Minutes** **1 - 10**  
To confirm the minutes of the meeting held on 16 December 2015 (copy attached).
- 3. Disclosures of Interest**  
To receive any disclosures of interest from councillors under the Councillors' Code of Conduct, or contact with applicants/objectors under the Planning Code.
- 4. Planning Applications and other Development Control matters**  
To consider and determine the planning applications and other development control matters in the report of the Head of Planning and Housing Strategy (copy attached).

  - a) 15/01590/SCC - Grazing Land Opposite Ford Close, Kingston Road, Ashford **11 - 30**
  - b) 15/00676/FUL - Land Off, Hanworth Road, Sunbury On Thames **31 - 54**
  - c) 15/01513/FUL - Headline House, Stanwell Road, Ashford **55 - 78**
  - d) 15/01352/FUL - Venture House, 42 - 54 London Road, Staines-upon-Thames **79 - 98**
  - e) 15/01343/HOU - 32 Maryland Way, Sunbury On Thames **99 - 108**
- 5. Standard Appeals Report** **109 - 112**  
To note the details of the Standard Appeals Report.
- 6. Urgent Items**  
To consider any items which the Chairman considers as urgent.

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**Minutes of the Planning Committee  
16 December 2015**

**Present:**

Councillor R.A. Smith-Ainsley (Chairman)  
Councillor C.M. Frazer (Vice-Chairman)

**Councillors:**

R.O. Barratt	V.J. Leighton	R.W. Sider BEM
S.J. Burkmar	A. Neale	H.A. Thomson
Q.R. Edgington	O. Rybinski	

**Apologies:** Apologies were received from Councillor A.E. Friday, Councillor A.L. Griffiths, Councillor N. Islam and Councillor A.T. Jones

**In Attendance:**

Councillors who are not members of the Committee, but attended the meeting and spoke on an application in or affecting their ward, are set out below in relation to the relevant application.

**298/15 Minutes**

The minutes of the meeting held on 18 November 2015 were approved as a correct record.

**299/15 Disclosures of Interest**

**a) Disclosures of interest under the Members' Code of Conduct**

The Chairman explained that in accordance with paragraph 27 of the Council's Planning Code under Part 5 Section (d) of the Council's Constitution applications 15/01395/HOU - 17 Rosefield Road, Staines-upon-Thames and 15/01442/HOU - Boundary House, 7 The Wickets, Ashford, TW15 2RR had been reported to the Committee.

Councillors R.A. Smith Ainsley, C.M. Frazer, R.O. Barratt, S.J. Burkmar, V.J. Leighton, A. Neale, O. Rybinski, R.W. Sider BEM, H.A. Thomson and Q.R.

Edgington declared a conflict of interest in relation to application 15/01395/HOU - 17 Rosefield Road, Staines-upon-Thames, TW18 4NB because the applicant Mrs J. Sexton was a fellow Conservative Councillor. They all confirmed that they had maintained an impartial role and had not expressed any views. They therefore took part in the debate and voted on the item.

Councillors R.A. Smith Ainsley, C.M. Frazer, R.O. Barratt, S.J. Burkmar, V.J. Leighton, A. Neale, O. Rybinski, R.W. Sider BEM, H.A. Thomson and Q.R. Edgington declared a conflict of interest in relation to application 15/01442/HOU - Boundary House, 7 The Wickets, Ashford, TW15 2RR because the applicant Mr Terry Cheshire was an employee of the Council. All councillors present confirmed that they had maintained an impartial role and had not expressed any views on the application. They therefore took part in the debate and voted on the item.

#### **b) Declarations of interest under the Council's Planning Code**

Councillors R.A. Smith Ainsley, C.M. Frazer, R.O. Barratt, S.J. Burkmar, A.E. Friday, V.J. Leighton, A. Neale, O. Rybinski, R.W. Sider BEM and Q.R. Edgington reported that they had received correspondence in relation to application 15/01206/RVC - The Workshop At The Boat Yard, The Boathouse, Sandhills Meadow, Shepperton, TW17 9HY but had maintained an impartial role, had not expressed any views and had kept an open mind.

#### **300/15 15/01243/SCC - Brett Aggregates Limited, Hithermoor Quarry, Leylands Lane, Stanwell Moor, Staines-upon-Thames, TW19 6AZ**

##### **Description:**

The construction and use of a soil treatment facility within the existing site.

##### **Additional Information:**

The Assistant Head of Planning informed the Committee there had been a consultation response from the Environmental Health Officer raising no objection on noise grounds.

##### **Public Speaking:**

There was no public speaking.

##### **Debate:**

During the debate the following key issues were raised:

- One councillor had personal experience elsewhere of this facility and it worked well.
- Concern over inadequate wheel washing facilities for HGVs leaving the site.
- Concern over adequacy of lagoon.

As Councillor Q.R. Edgington arrived after the debate had begun he did not participate or vote on the item.

**Decision:**

The consultation response was modified as set out below:

That Surrey County Council be informed that this Council **OBJECTS** to the current proposal for the construction and use of a soil treatment facility within the existing site, unless the following matters are satisfactorily addressed:

- a) The imposition of a condition requiring the HGV movements associated with the soil treatment facility to be no greater than the overall number of movements as permitted within the existing permission;
- b) The completion of an updated Dust Action Plan; and the imposition of associated conditions requiring the measures outlined in the Dust Action Plan and Air Quality Assessment are implemented;
- c) Confirmation from the applicant that the installation of the surface water lagoon has sufficient capacity to contain all runoff associated with the treatment facility.
- d) Adequate wheel washing facilities for HGVs are installed at the site and enforced.

**301/15 15/01206/RVC - The Workshop At The Boat Yard, The Boathouse, Sandhills Meadow, Shepperton, TW17 9HY**

**Description:**

Variation of condition 5 of planning permission ref E/88/767 (referring to the use of the building only for purposes ancillary to the existing boat hire business) to allow for the protective treatment of vehicles within the workshop area.

**Additional Information:**

The Assistant Head of Planning informed the Committee that there was an amendment to the description of the proposal in the published agenda papers which referred to the use of the building not site. She explained that the description of the proposal should read:

*Variation of condition 5 of planning permission ref E/88/767 (referring to the use of the building only for purposes ancillary to the existing boat hire business) to allow for the protective treatment of vehicles within the workshop area.*

The Assistant Head of Planning explained that an amended site plan had been submitted by the applicant which deleted reference to any storage of car parking within the site.

In addition a late consultation response had been received from the Chair of the Sandhill's Meadow and Las Palmas Estate Residents Association which raised the following points:

- Video clips were attached showing noise created at the workshop in the summer.
- Site will change from light industrial to general industrial use
- Incompatible with a residential area
- unacceptable levels of noise and parking
- Conditions are unenforceable.

The Assistant Head of Planning recommended that the following conditions be amended:

1. The development hereby permitted shall be carried out in accordance with the following approved plans: location plan received on 08/09/2015 and block plan received on 16/12/2015.

Reason: For the avoidance of doubt and in the interest of proper planning.

3. All works undertaken within the development hereby approved are to be undertaken within the workshop with the doors closed.

Reason:

To ensure that the proposed development does not prejudice the enjoyment of neighbouring occupiers of their properties in accordance with policies SP6, EN1 and EN11 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

4. The noise from the use hereby approved must be 5 dB below background at the nearest noise sensitive premises.

Reason:

To ensure that the proposed development does not prejudice the enjoyment of neighbouring occupiers of their properties in accordance with policies SP6, EN1 and EN11 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

6. That the use hereby approved, in regards to works on motor vehicles, shall be limited to the protective treatment involving steam cleaning and polishing.

Reason: - To protect the amenities of the local area.

7. That the use hereby approved for the protective treatment of vehicles shall be limited to no more than 49% of the hours worked on motor vehicles and the additional hours worked shall be on boats. The applicant shall submit details to the LPA during the months of March and September of works undertaken to comply with this condition.

Reason: -

To ensure at least 49% of the hours worked are carried out on boats which would be ancillary to the boat yard use.

The Assistant Head of Planning recommended that that the following Additional condition be included:

- The permission hereby approved shall be limited to a temporary period of 1 year expiring on 16/12/2016; when the use hereby approved shall be discontinued.

Reason: In the interests of the amenity of the area and to allow the use to be monitored.

**Public Speaking:**

In accordance with the Council's procedure for speaking at meetings David Grant spoke against the proposal raising the following key points:

- Change of use from light to general industrial use
- Concern with impact of day to day activities especially in the summer
- Parking on roadway outside workshop causing danger and inconvenience
- The painting of the building black and the adverts are out of character with the surrounding area.

In accordance with the Council's procedure for speaking at committee meetings, Michael Cook spoke for the proposed development and raised the following key points:

- Accepts the report and the conditions (as amended)
- Conditions achieves the right balance between residential amenity and the needs of the business
- Boatyard has been on the site for over 100 years

**Debate:**

During the debate the following key issues were raised:

- Strong conditions are imposed
- Proposal is for a one year temporary permission
- Is within the Greenbelt, Plotlands, Area of Special Character (Officer note: it is not within the Area of Special Character but is within the Flood Plain).
- It is a mixed area not just a residential area
- If the conditions are not adhered to over the year, this will be taken into account if permission is sought to renew
- Building should be painted white
- Concern with enforcing conditions
- Should start at 9am on Saturdays not 8am (a motion to change the condition was lost)
- Noise has not been a problem in the past/noisy activity
- If it goes to appeal, they could work any hours they wish
- Need to protect amenity of residents not business sustainability
- Concern over steam cleaning in summer with doors closed
- Sandhills Meadow is a private road

- Surrounding dwellings are close
- Use is not ancillary to the boatyard
- Concerns over vehicle activity
- Ceramic Pro is a worldwide franchise.

An amendment to condition 2 was moved by Councillor Q.R. Edgington and seconded by Councillor O. Rybinski to read:

*"2. That the premises are not used for the purposes hereby permitted before 08.00 or after 18.00 on Monday to Fridays or before 09.00 and 13.00 on Saturdays and not on Sundays or Bank Holidays.*

*Reason: To ensure that the proposed development does not prejudice the enjoyment of neighbouring occupiers of their properties in accordance with policies SP6, EN1, EN11 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009."*

The amendment was lost.

**Decision:**

The application was **approved** as set out in the report of the Head of Planning and Housing Strategy subject to the following amendments to conditions and one additional condition as set out in the additional information as follows:

2. The development hereby permitted shall be carried out in accordance with the following approved plans: location plan received on 08/09/2015 and block plan received on 16/12/2015.

Reason: For the avoidance of doubt and in the interest of proper planning.

3. All works undertaken within the development hereby approved are to be undertaken within the workshop with the doors closed.

Reason:

To ensure that the proposed development does not prejudice the enjoyment of neighbouring occupiers of their properties in accordance with policies SP6, EN1 and EN11 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

4. The noise from the use hereby approved must be 5 dB below background at the nearest noise sensitive premises.

Reason:

To ensure that the proposed development does not prejudice the enjoyment of neighbouring occupiers of their properties in accordance with policies SP6, EN1 and EN11 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

6. That the use hereby approved, in regards to works on motor vehicles, shall be limited to the protective treatment involving steam cleaning and polishing.

Reason: - To protect the amenities of the local area.

7. That the use hereby approved for the protective treatment of vehicles shall be limited to no more than 49% of the hours worked on motor vehicles and the additional hours worked shall be on boats. The applicant shall submit details to the LPA during the months of March and September of works undertaken to comply with this condition.

Reason: -

To ensure at least 49% of the hours worked are carried out on boats which would be ancillary to the boat yard use.

**Additional Condition:**

- The permission hereby approved shall be limited to a temporary period of 1 year expiring on 16/12/2016; when the use hereby approved shall be discontinued.

Reason: In the interests of the amenity of the area and to allow the use to be monitored.

**302/15 15/00977/FUL - 36B Kingston Road, Staines-upon-Thames, TW18 4LN**

**Description:**

Installation of temporary car wash facility to Staines Tyres existing forecourt.

**Additional Information:**

The Assistant Head of Planning informed the Committee that an amended plan had been received from the applicant and as a consequence condition 1 was amended to reflect the plan as follows:

*The development hereby permitted shall be carried out in accordance with the following approved plans: site location plan received on 15/07/2015, drawing no 15-039/A3/SK004 P1 received on 06/10/2015 and 15-039/A3/SK003 P3 received on 02/12/2015.*

**Public Speaking:**

There was no public speaking.

**Debate:**

During the debate the following key issues were raised:

- The proposal was refused previously because of water pollution but this has now been resolved.

**Decision:**

The application was **approved** as set out in the report of the Head of Planning and Housing Strategy subject to the following amendment to condition 1:

The development hereby permitted shall be carried out in accordance with the following approved plans: site location plan received on 15/07/2015, drawing no 15-039/A3/SK004 P1 received on 06/10/2015 and 15-039/A3/SK003 P3 received on 02/12/2015.

**303/15 15/01395/HOU - 17 Rosefield Road, Staines-upon-Thames, TW18 4NB**

**Description:**

Erection of a part two storey, part single storey side and rear extension and installation of front and rear dormers to create a loft conversion.

**Additional Information:**

There was none.

**Public Speaking:**

There was no public speaking.

**Debate:**

During the debate the following key issues were raised:

- The proposal has already been approved but this is for a smaller scheme.

**Decision:**

The application was **approved** subject to the conditions and informatives as set out in the report of the Head of Planning and Housing Strategy.

**304/15 15/01442/HOU - Boundary House, 7 The Wickets, Ashford, TW15 2RR**

**Description:**

Erection of a two storey side extension and single storey rear extension.

**Additional Information:**

There was none.

**Public Speaking:**

There was no public speaking.

**Debate:**

During the debate the following key issues were raised:

- Proposal complies with policy
- We take particular care with Council Officer applications
- No adverse impact on street scene



**Decision:**

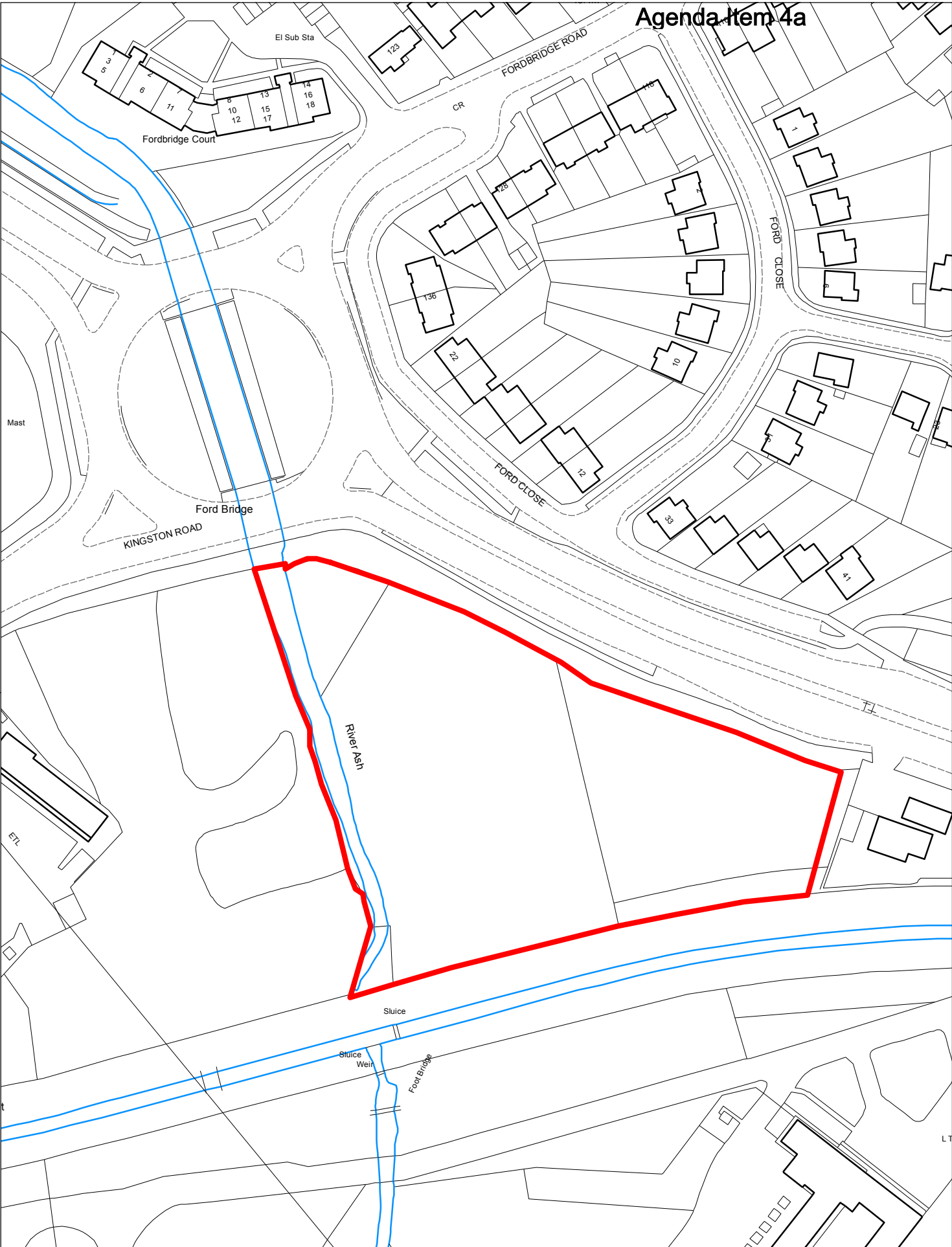
The application was **approved** subject to the conditions and informatives as set out in the report of the Head of Planning and Housing Strategy.

**305/15 Standard Appeals Report**

The Chairman informed the Committee that if any Member had any detailed queries regarding the report on Appeals lodged and decisions received since the last meeting, they should contact the Head of Planning and Housing Strategy.

**Resolved** that the report of the Head of Planning and Housing Strategy be received and noted.

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**15/01590/SCC**  
**Grazing land opposite Ford Close,**  
**Kingston Road, Ashford**

1:1,250

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<b>Application No.</b>	15/01590/SCC		
<b>Site Address</b>	Grazing land opposite Ford Close, Kingston Road, Ashford		
<b>Proposal</b>	Surrey County Council consultation for the construction of new single storey fire station with access from A308 Staines Road West, incorporating two double appliance bays, dormitories with ancillary facilities, office accommodation, operational areas and store rooms; drill tower and smoke house; proposed hard standing for training, car parking and refuelling point for appliances; associated generator and oil storage tank; retention of existing rail timber fencing on north and eastern boundary of the site and the erection of 3m high acoustic fencing on the south, west and part of the northern boundaries.		
<b>Applicant</b>	Surrey County Council .		
<b>Ward</b>	Laleham and Shepperton Green		
<b>Call in details</b>	N/A		
<b>Case Officer</b>	Peter Brooks		
<b>Application Dates</b>	Valid: 23.11.15		
<b>Executive Summary</b>	<p>The application site is a triangular piece of land to the south of the Fordbridge roundabout. To the west of the site lies an open grazing field, the river Ash and the Council Depot. To the east lies a car wash facility. The land is open in character and forms part of the designated a Green Belt.</p> <p>The application is a consultation from Surrey County Council (SCC) for the erection of a fire station building with associated facilities. This fire station would serve the borough of Spelthorne and would replace the two existing fire stations within the borough (one opposite Ashford Hospital and one close to Sunbury Cross).</p> <p>The single fire station has been identified by Surrey Fire and Rescue Service (SFRS) as a suitable replacement for the two existing fire stations within the borough. Spelthorne must assess the proposal in planning terms. This site has been identified as the most suitable site within the borough for a new fire station, based on a number of criteria applied by the SFRS. It is not for the planning process to review the merits of the operational implications of the proposal.</p> <p>The fire station would comprise a single storey structure located close to the northern boundary of the site, fronting the A308. The element closest to the road would be relatively low profile, and contain offices, accommodation for on duty staff and various ancillary rooms. To the rear</p>		

	<p>of the building is the fire engine house, which has a higher roof to accommodate the fire fighting vehicles. To the west, south and east of the main building is a hard standing area for parking and training. The hard standing area to the south contains a training tower, generator and fuel tank to serve the fire building. The operational needs to the SFRS dictate the layout and form of the proposed fire station.</p> <p>In view of the site's location in the Green Belt, it is accepted that the construction of a new building would be viewed as 'inappropriate' development which would represent a departure from the Council's 'development plan'. However, the applicant's have put forward a strong case which demonstrates a series of 'very special circumstances' that justifies the erection of this particular building on this site.</p>
<p><b>Recommended Decision</b></p>	<p>It is recommended that Spelthorne Borough Council raise <b>OBJECTIONS</b> to this proposal, unless it can be demonstrated that it would be able to overcome flooding and landscaping concerns.</p>

## MAIN REPORT

### 1 DEVELOPMENT PLAN

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

- GB1 - Saved Local Plan Policy (Green Belts)
- CO1- Providing Community Facilities
- LO1 - Flooding
- EN1 - Design of New Development
- EN3 - Air Quality
- EN7 - Tree Protection
- EN13 - Light Pollution
- EN15 - Development on Land Affected by Contamination
- CC2 - Sustainable Travel
- CC3 - Parking Provision

### 2 RELEVANT PLANNING AND OTHER HISTORY

2.1 This site has no relevant planning history. It has been used as grazing land for many years and was previously owned and leased for that use by the Borough Council.

### **3 DESCRIPTION OF CURRENT PROPOSAL**

- 3.1 This is a consultation on a planning application which has been submitted by Surrey County Council (SCC), seeking the views of Spelthorne Borough Council (SBC) on the proposal to construct a fire station on the application site.
- 3.2 Surrey Fire and Rescue Service (SFRS) have, as part of a wider review, considered that to meet its current and future operational requirements, it needs to construct a new single fire station in the Borough and this location is the best in terms of access and response times. It will then close the two existing fire stations within the borough (one of which is located opposite Ashford Hospital and one which is located close to Sunbury Cross).
- 3.3 Spelthorne must make its recommendation based upon planning matters and not matters relating to the relative merits of closing the two existing stations. SFRS are the body who determines the operational need within the County.
- 3.4 The application site is a triangular piece of land to the south east of Fordbridge Roundabout. To the west of site lies an open grazing field, the river Ash and further west is the Council depot. To the east lies a car wash facility. The site is open in character and forms part of the designated Green Belt.
- 3.5 Also forming part of the application site is an existing access which runs to the south of the site and forms part of the access to the Bretts Queen Mary Reservoir minerals site. This access is from Ashford Road and runs west to east, and exits onto Staines Road West (A308). This access would be used to allow rapid access to the proposed fire station site by SFRS staff.
- 3.6 The fire station building would be a single storey structure with its longest axis running parallel to Staines Road West (this element would be 51m x 11.3m and 4.6m high). The building would be located on the northern edge of the site, with hard standing to its west, east and south. This part of the building would contain a number of rooms which would serve the fire station. These include rooms for on duty staff to sleep, a kitchen area, recreation rooms, bathroom/shower facilities, general office space and rooms for storing/cleaning/maintaining firefighting equipment, as well as other ancillary rooms. The remainder of the main building (33.5m x 15.7m) would comprise an appliance bay where the fire fighting vehicles would be stored. This appliance bay has a higher roof in order to accommodate the vehicles, and would have a maximum height of 7.765m. The roof over the building would comprise two monopitch elements in order to reduce its visual impact. The building would be of functional and simple appearance, with a brick exterior and metal roof.
- 3.7 The hard standing area which would be located on the east, south and west of the site would be used for car parking, training, and would allow manoeuvring space for fire vehicles. To the south of the main building a training tower/rescue house (a four storey structure used for training purposes) would be located with a total height of 12.27m. There would also be a generator and fuel tank to meet operational needs of the site in times of emergency
- 3.8 The design of the fire station is to a great extent dictated by its functional requirements, and so its form and layout reflects the needs of the SFRS.

- 3.9 The proposal would also involve alterations to the existing highway to allow fire fighting vehicles to exit the site and turn east onto Staines Road West (the A308) in order to avoid delays in reaching emergency calls. The proposal would remove part of the existing central reservation and a crossover would be created. Lights would be installed on the A308 to halt traffic and allow fire vehicles to exit the site in emergencies. The proposal would also include the use of an existing access road which runs to the south of the site between Ashford Road and the A308. This access would allow returning engines and ‘on call’ firefighting staff to rapidly enter the site when travelling from the south. A further explanation of the ‘on call’ fire fighters is provided later in the report.
- 3.10 Copies of the proposed site layout and elevations are provided as an Appendix.

#### **4 CONSULTATIONS**

- 4.1 The following table shows those bodies consulted and their response

<b>Consultee</b>	<b>Comment</b>
<b>County Highway Authority</b>	Raises no objections.
<b>County Archaeological Officer</b>	Will respond to applicant.
<b>Environment Agency</b>	Will respond to applicant.
<b>The Council’s Tree Officer</b>	Verbally reported no objections subject to improved landscaping.
<b>Environmental Health</b>	No objections subject to conditions relating to contamination and gas protection.

#### **5 PUBLIC CONSULTATION**

As Spelthorne is a consultee SCC have notified adjoining neighbours. At the time of writing this report, the Council has not been passed any letters of representation from SCC, and has received one letter directly concerning the scheme.

#### **6 PLANNING ISSUES**

- Green Belt
- Flooding
- Community Facility
- Design, Appearance and Visual Impact
- Highway Issues and Parking
- Residential Amenity
- Noise
- Light Pollution
- Contamination
- Archaeology

## 7 PLANNING CONSIDERATIONS

### Green Belt

#### a) Background

- 7.1 The site lies within a designated Green Belt and Saved Local Plan Policy GB1 is most relevant as it seeks to ensure only 'appropriate' development is allowed in the Green Belt. The National Planning Policy Framework (NPPF) states that the construction of new buildings should be regarded as 'inappropriate' development, which by definition would be considered as harmful to the Green Belt and should not be approved except in very special circumstances. The NPPF continues by stating that:-

*'When considering any planning application, local planning authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very special circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and other harm, is clearly outweighed by other considerations'.*

The following paragraphs assess the proposal on the above basis having regard to the harm to the Green Belt.

#### b) Harm to the Green Belt

- 7.2 It is accepted that this development would therefore not be deemed as "appropriate" in policy terms when having regard to the NPPF and the Planning Policy Guidance Note on Green Belts.
- 7.3 Given the open character and undeveloped nature of the application site, it is acknowledged that any new building would clearly affect the open character of the existing Green Belt. It is clear that the fire station building and the other structures within the site would be visible within the Green Belt which would result in some visual harm to the Green Belt setting.

#### c) Very Special Circumstances

- 7.4 The applicants have set out in some detail the 'very special circumstances' which apply in this case and which are summarised below. These are as follows:
1. **The need for development:** SFRS have identified that there is a lack of up to date facilities serving the Spelthorne area. This conclusion followed a strategic review conducted across Surrey in 2010. This review concluded that a new single fire station in Spelthorne is necessary. In order to provide a more balanced service provision across the county which would be able to achieve the Surrey Response standard (that being in attendance to 80% of all critical incidents with either one appliance in 10 minutes or two appliances in 15 minutes).
- Following this review five options were available 1) do nothing to secure improvements 2) Close Sunbury and maintain Staines 3) Close Staines and maintain Sunbury 4) Open a new fire station with one 24 hour whole-time engine 5) Open a new fire station with one 24 hour and on on-call fire



engine. Based on a report titled “Facing the future” which looked at whether ‘on-call’ fire fighters could meet operation need, SFRS considers that the use of both full time and ‘on-call’ fire fighters would be suitable in this position. (note: On-call fire fighters are trained to the same standard as full time fire fighters, and work in other trades. They respond to emergency calls when required and respond in a similar way to the Coastguard and Mountain Rescue services).

Modelling was undertaken to determine the optimum location for the new fire station, and a ‘heat map’ was produced showing the area best suited to locate the new station. The Ashford area was shown to be the best area due to its relatively central location within the borough, its accessibility and that no suitable sites could be located within the existing urban area. A number of sites were considered, but none were found to be suitable.

2. **No other suitable sites available:** In order to determine the best site for the new fire station, a number of criteria were set out. These were:
  - Location/Response time – site should be located in most advantageous position to assist response times for 999 incidents, and to assist ‘on-call’ staff in reaching the site.
  - Size – the site must be of a sufficient size to accommodate 2 fire engines and associated facilities. As such a minimum site area of 0.6 hectares is required
  - Adjacencies – the fire station should not be located adjacent to residential properties due to noise and disturbance from training and responding to incidents
  - Availability – the site must be available, and preferably under full control of the applicant
  - Topography – site must be as level as possible to facilitate a variety of training methods
  - Access – excellent access should be available to the highway network in several directions
  - Planning Policy designations – appropriate planning designations that allow for development or which can be presented or require tests to be satisfied in order to make the development appropriate
  - Existing use – site must not be in a use which prevents the proposed use.

Using the above criteria SFRS assessed a total of 5 sites, giving each site a total score. Only the application site scored highly enough to be able to satisfy the requirements of the SFRS. The site at Fordbridge roundabout was able to satisfy the requirements as follows:

Location/Response times – The heat map produced by the study centred on Ashford urban area, this was however an unsuitable area due to its density and associated traffic congestion to locate a fire station. The proposed site was located on the south west corner of the ‘heat map’, and is well connected to the main highway network. This will allow better response times.

Size – the application site would be larger than the minimum required 0.6 Hectares. The site would be 0.84 hectares and so provide over sufficient space to meet operational needs.

Adjacencies – the site would not directly adjoin any residential properties. The nearest properties are located in Ford Close to the north over the A308 (approximately 40m), and it is considered this separation by a main trunk road would not significantly harm the amenities of the occupiers of these dwellings.

Availability – SCC owns the entire application site.

Topography – the site is level.

Access – the sites location accessing straight onto the A308 dual carriage way would provide excellent access to the whole borough (eg on the A308 dual carriageway from Staines to Sunbury, access via the A308 to the A30 dual carriageway, Ashford Road leading south to Laleham and Shepperton . The provision of a separate access to the south of the site to allow engines and ‘on-call’ to staff to access the site when travelling from the south also weighs in favour of the schemes accessibility.

Planning policy designations – the site is located within the Metropolitan Green Belt and a medium probability flood risk area. The application requires the submission of a Very Special Circumstances Case (in relation to Green Belt) and the completion of a sequential and exception test (in relation to flooding). The applicant has been able to demonstrate that the scheme would therefore comply with planning policy.

Existing use – the site is currently used as grazing land and is available for development, without the need for extension preparatory works.

3. **Poor quality existing fire stations:** The two existing fire stations date from the mid 1960’s and suffer from a number of issues relating to their age, as well as their historical design. The buildings are not of a suitable size to meet modern standards, and the sites lack suitable space to conduct training of a modern standard. The sites are in simple terms, not of a suitable size to redevelop for a fire station, and the sites would not allow the SFRS to provide the quality of service to comply with Core Functions as identified in the Fire and Rescue Services Act 2004.

The existing buildings due to their age require extensive works to bring them up to modern standards, and their construction utilising large elements of flat roof and large glazed windows results in buildings that are expensive to run. Therefore for both operational and economic reasons it was recommended that the two existing stations be closed and a single new station, built to meet modern standards be constructed.

4. **Centrally located site required:** In order to meet required response times (outlined in paragraph 7.4 (1)) the site should be located within the ‘very good’ category on the ‘heat map’. The site would therefore be located to allow the best response times for emergency vehicles. The sites location, being central to the borough, would constitute a very special circumstance weighing in favour of the scheme.

### **Summary of the Very Special Circumstances**

5. The proposal would constitute inappropriate development which would be harmful to the Green Belt. However the harm would be outweighed by the operation need of the SFRS to provide a new, modern fire station which

can meet the needs of the borough. The existing fire stations, due to their age and size, cannot provide facilities that meet modern requirements, and their sites are so constrained that they could not be redeveloped. The proposed station would allow the SFRS to meet its response time targets and would provide modern facilities to allow up to date training. The proposal has therefore been able to demonstrate that very special circumstances have been demonstrated, and that the harm to the green belt is outweighed by the need to provide a new fire station within the borough.

## Flooding

### A) Background

- 7.5 The site lies within Flood Zone 2 (FZ2) which is defined as a medium probability flood zone (a 1 in 1000 year flood event area), and the primary flood risk is from the River Ash, which forms the western boundary of the site. A fire station is classed as a 'highly vulnerable' use due to it being required during times of flood. Ideally one would not locate a fire station in FZ2. The Flood risk vulnerability and flood zone 'compatibility' table (as defined in the National Planning Policy Guidance – NPPG) identifies that in order to be classed as compatible 'highly vulnerable' uses must pass the Sequential and Exception test
- 7.6 The proposed fire station site and building would be raised above the expected flood level to safeguard it during times of flood, and further details of this are outlined in paragraph 7.13.

### B) Sequential and Exception Test

- 7.7 Paragraphs 100 to 103 in the NPPF set out how applications should be steered away from those areas liable to flood, and if this is not possible, the tests they must go through to demonstrate that they would not make the overall flooding situation worse and they would provide an overall benefit to the area
- 7.8 The sequential test aims to steer new development to those areas least liable to flood. Development should not be permitted if there are reasonable available sites appropriate for the proposed development in areas with a lower probability of flooding. If no other sites can be identified then the proposal must pass the Exception test.
- 7.9 The Exception test comprises two parts. It must be able to demonstrate that the development would provide wider sustainability benefits to the community that outweigh the flood risk, and that the development would be safe for its lifetime taking into account the vulnerability of its users, without increasing flood risk elsewhere.

### C) The Sequential Test

- 7.10 As has been demonstrated as part of the Green Belt Very Special Circumstances case, the proposed site is deemed to be the most appropriate site within Spelthorne to locate the proposed fire station. When applying the

sequential test, a pragmatic approach on the availability of alternative sites should be taken. As this site has been concluded to be the most appropriate site, there are no other reasonable available sites with a lower probability of flooding than FZ2 and as such the sequential test is considered to be passed acceptably.

#### D) The Exception Test

- 7.11 The exception test requires that applicants demonstrate that the scheme would have wider sustainability benefits to the community that outweigh flood risk, and that the development would be safe for its lifetime, taking into account the vulnerability of its users, without increasing flood risk elsewhere.
- 7.12 The applicant has explained that the provision of a new fire station would benefit the wider community by virtue of improving the SFRS to be able to respond to emergency situations. It is considered, based on the evidence submitted, that the proposed fire station would be better placed to deal with emergencies within the borough, and the wider area, by virtue of its position to be able to have a 'very good' ability to serve the response area.
- 7.13 The Flood Risk Assessment (FRA) submitted with the application recommended that the site be finished higher than the predicted flood level (1 in 100 years plus climate change) to a level of 13.47 AOD. The finished floor level of the fire station would be set at 14.20 AOD, to provide a freeboard of 0.73m. As the use as a fire station would be classed as highly vulnerable it is considered the raised floor level and dry means of escape to the east, would mean the users would be kept safe during times of flood. The lifetime of non-residential is dependent on the characteristics of the proposed development. The FRA has demonstrated that by virtue of its raised level, and provision of Sustainable Urban Drainage (SUDs) incorporated in the scheme, the development would be 'safe' over its expected lifetime.
- 7.14 The raising of the ground level within the floodplain would reduce the area which would be capable of flooding ie reduce flood storage capacity. The applicant could address this concern by increasing the flood storage capacity on the site by lowering levels around the new station site to compensate for this.

#### E) Summary of flooding

- 7.15 The proposal has demonstrated that the sequential test has been passed, however it is considered the exception test has not been fully passed, as currently the proposal would increase flood risk elsewhere due to the loss of flood storage capacity. This can be overcome by increasing flood storage capacity on the undeveloped western edge of the site closest to the River Ash to compensate for the proposed loss. Without this change objection is recommended on flooding grounds.

#### Design, Appearance and Visual Impact

- 7.16 The fire station building and other supporting structures has been designed to minimise its visual impact in this green belt location. The design and form however has been dictated by the proposed use, and structures must be of a certain size to accommodate fire engines and space for on duty fire fighters

and training areas. It is considered the design and form have been calculated so as to minimise the impact on the character of the area.

### Transport Assessment

- 7.17 The application has been accompanied by a Transport Assessment (TA). This TA concluded the site was located in an accessible location (as supported by the heat map assessment) and is well linked to the wider transport network. The required visibility splays can be achieved and 'wig wag' signal lights would be installed to halt traffic on the A308 to allow fire engines to leave the site safely during emergency call outs. Such arrangements for emergency vehicles are not uncommon.
- 7.18 The response of the County Highway Engineer will be reported verbally.

### Construction Management

- 7.19 A construction management plan details how the scheme would be constructed, in a way to minimise disruption on the wider area. These documents are common for larger developments and it is considered the proposed would be acceptable.

### Arboricultural Considerations

- 7.20 The proposal would involve the removal of a total of 7 trees from the site (one class B, three class C and three class U), and some other would require some pruning. A number of trees would be maintained on the site, in particular the group at the eastern end of the site. None of the trees proposed to be removed are protected by a preservation order, and the site does not fall within a conservation area. It is not considered the removal of the trees would have an unacceptable impact subject to appropriate new planting. However, more extensive landscaping is required to soften the visual impact of what is a substantial building.

### Acoustic Considerations

- 7.21 The submitted noise impact assessment concludes that the acoustic impacts on the development have been investigated and found to be acceptable in this location. Noise impacts likely to be generated includes during emergency responses, training and the installed plant on site. The nearest residential properties are located to the north in Ford Close, and are separated from the site by the A308 dual carriageway.

### Air Quality

- 7.22 An air quality report submitted with the scheme concluded the additional traffic generated by the development at this specific location would not significantly affect air quality for the existing properties along this part of the local road network. It was also concluded that training activities would not have significant harmful effects on air quality. It should be noted that overall the air quality impact is little different to the 2 existing stations.

## Archaeology

- 7.23 A County Archaeological unit conducted a trial trench evaluation. From a total of seven trenches, within three traces of iron age/prehistoric pottery was found. It has been recommended that further work be undertaken to ascertain if further material can be located. The County Archaeologist will respond directly to the County Planning Officer.

## Ecology

- 7.24 The application has been accompanied by ecology studies which concluded that no significant protected species were found on site. It was noted that a small number of bats use the River Ash corridor to 'commute'. As the proposal would maintain a separation of 8m between the river bank and the boundary of the proposed fire station it was concluded that the proposal would not adversely impact commuting bats. Conditions can be added by the County to ensure that ecology is protected during construction, and if any is found during construction how to deal with this eventuality.

## Conclusion

- 7.25 The proposal has passed both the sequential test, but has not passed the exception test in relation to flooding due to the loss of flood storage capacity which could increase flood risk elsewhere. This matter could be overcome by increasing flood storage on site, and Spelthorne therefore currently object to the proposal on this basis.
- 7.26 The current landscaping scheme does not provide sufficient mitigation of the visual impact of what is a substantial building. By increasing landscaping around the building to reduce its visual impact, Spelthorne would be able to remove their objection on this issue.
- 7.27 In assessing the proposal in relation to the Green Belt, it has already been explained that substantial weight must be given to the harm to the Green Belt. It is also considered that the size of the structure means, notwithstanding proposed landscaping, the scheme would lead to moderate additional visual harm. Subject to the flooding and landscaping issues being resolved, I would be satisfied that the substantial case for a modern well located fire station to constitute 'very special circumstances' and would 'clearly' outweigh the Green Belt harm.
- 7.28 The other matters considered which have been referred above would be acceptable subject to several planning conditions.
- 7.29 The consultation for a new fire station from SCC is currently considered to be unacceptable, due to the loss of flood plain storage and insufficient landscaping surrounding the site, resulting in a position where the harm to the Green Belt is not 'clearly outweighed'. It is therefore recommended that SCC be informed that Spelthorne Borough Council raise Objections to the proposal unless flood storage capacity on site is increased and by incorporating a more comprehensive landscaping scheme to better screen the development.

## **8 RECOMMENDATION**

8.1 That Surrey County Council be informed that Splethorne Borough Council raise OBJECTIONS to the proposed new fire station to be located on land south of Fordbridge Roundabout unless:

1. Flood storage capacity is increased to avoid adding to flood risk elsewhere; and
2. Sufficient landscaping is proposed to mitigate the visual impact of the proposal

Spelthorne would also comment that if permission was to be granted by SCC then the following should be satisfied:

1. Adequate protection of ecology during construction;
2. A demonstration that impact on archaeology is acceptable;
3. Acceptable in terms of highway safety to the satisfaction of the County Highway Authority.
4. Air quality measures presented in the Air Quality Assessment be controlled by condition and that Construction Environmental Management Plan and Dust Management Plan be approved prior to commencement to protect human health and prevent nuisance.
5. Ground gas mitigation of ground gas risk be approved prior to commencement and the scheme be constructed in accordance with these approved details.
6. That the following conditions be applied in relation to ground contamination:

**A) Condition:** No development shall take place until:-

(i) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site, has been submitted to and approved in writing by the Local Planning Authority.

(ii) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.

(iii) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

**Reason:** To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

#### NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination:Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

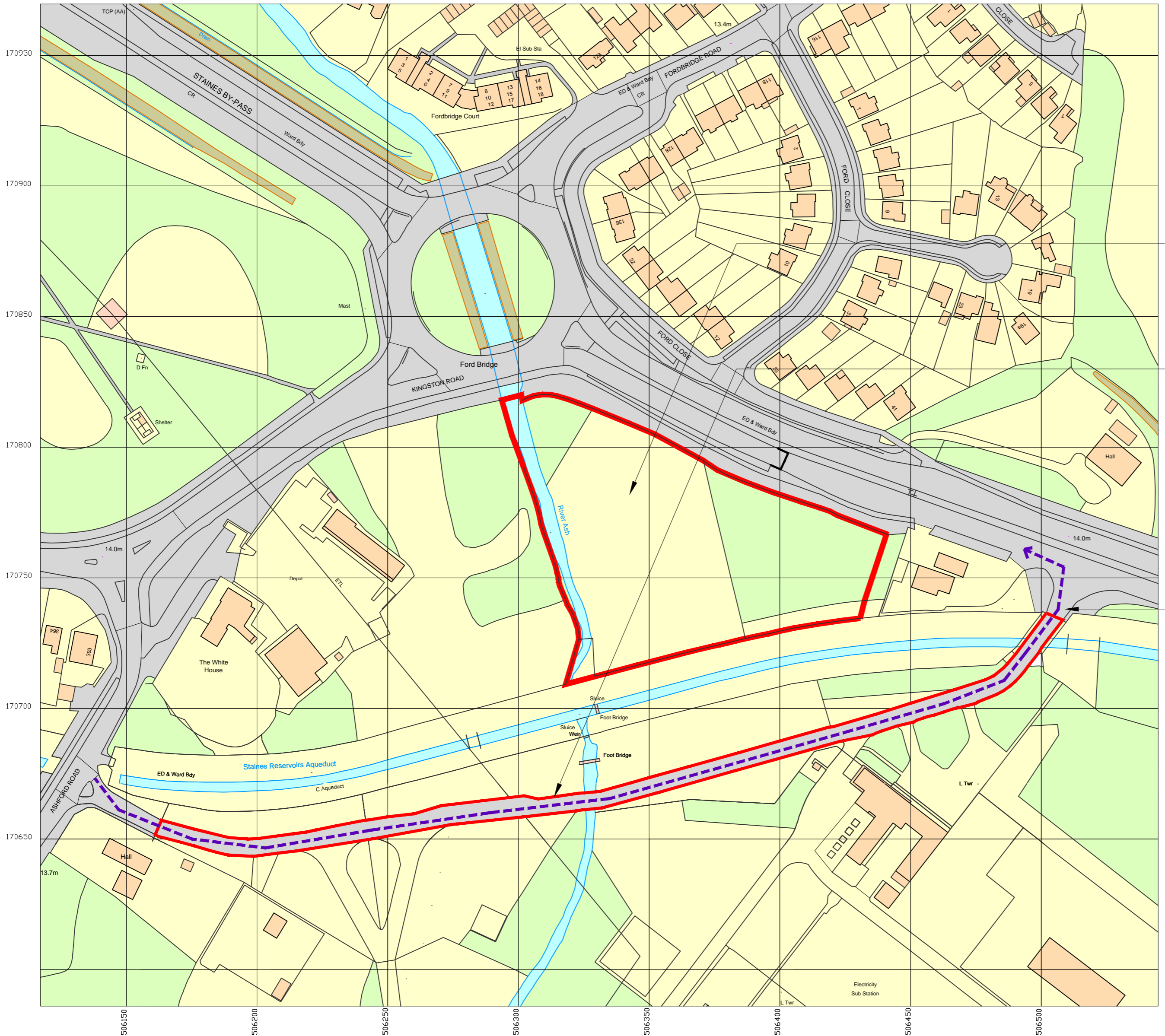
**B) Condition:** Prior to the first use or occupation of the development, and on completion of the agreed contamination remediation works, a validation report that demonstrates the effectiveness of the remediation carried out shall be submitted to and agreed in writing by the Local Planning Authority.

**Reason:** To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

#### NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination:Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

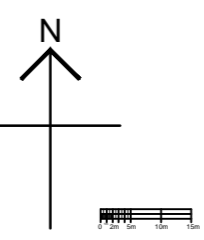




Proposed Site for the New Spelthorne Fire Station. Note: This is a Flood Zone Risk 2 and designated Green Belt area

Route indicated in purple is along an existing Thames Water roadway linking between the A308 and Ashford Road. This route will be for Fire Appliances and 'On Call' Fire Crews to use so as to gain faster access to the new crossover of the Fire Station (on the northbound carriageway of the A308) when approaching the site from north of the Fordbridge roundabout.

Note: Due to 'On Call' Fire Crews only having a 5 minute period in which to reach the Fire Station and mobilise the Appliance use of this roadway will avoid the potential for critical time being lost heading southwards down the A308 to the M3 roundabout before turning round and heading back to the site. 'On Call' Crews located North of the site responding to a mobilisation call will therefore exit the Fordbridge roundabout via Kingston Road/Ashford Road and use the Thames Water roadway (outlined in red) to cross south of the Fire Station site and return a short distance north on the A308 into the site. Note: Access Rights across this private Thames Water roadway have been agreed with legal agreements currently being finalised by Surrey County Council



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Amendments		
P1	Planning Application Issue	01/10/15
Client: Surrey County Council/Surrey Fire & Rescue		
Project: Proposed New: <b>Spelthorne Fire Station</b> Fordbridge Roundabout/A308 Staines By-Pass, Surrey, TW15 3SL		
Stage: Planning		
Sheet Title: <b>Location Plan</b>		
Drawn By: JLB	Checked By: RSP	
Job / Drawing No.	<b>7834.P.100</b>	Rev: PI
Date: 23/06/2015	Scale: 1:1250@A2	

**Raymond Smith Partnership LLP**  
 The Old Bank 5 High Street Old Town  
 EASTBOURNE East Sussex BN21 1HG  
 Tel: 01323 727788 Fax: 01323 726644  
 Mob1: 07785 762272 Mob2: 07785 762273

e-mail: info@rsp-architects.co.uk web: www.rsp-architects.co.uk  
**PLAYLE & PARTNERS LLP**  
 CONSTRUCTION & PROPERTY CONSULTANTS  
 Crest House • 138 Main Road • Sidcup • Kent • DA14 6NY  
 Tel: 020 8300 6811 web: www.playleandpartners.co.uk













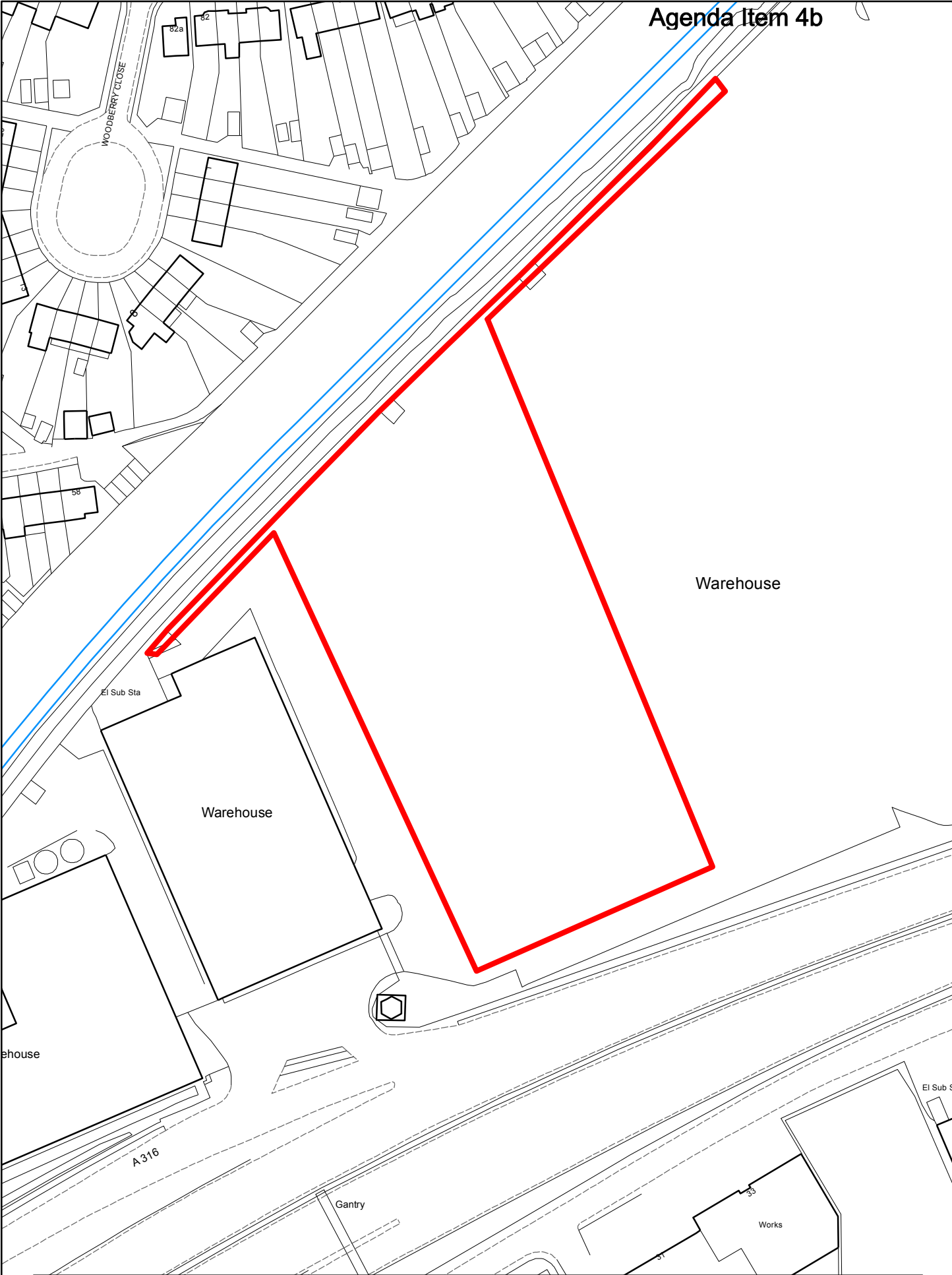












Warehouse

Warehouse

El Sub Sta

A 316

Gantry

Works



**15/00676/FUL**  
**Hanworth Road, Sunbury-on-Thames**



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**Planning Committee**

**13 January 2016**



Application Nos.	15/00676/FUL		
Site Address	Land off Hanworth Road, Sunbury on Thames, TW18 2HF		
Proposal	Erection of new building to provide a B1c, B2 and B8 development with associated parking		
Applicant	Diageo c/o Graftongate Investments		
Ward	Sunbury Common		
Call in details			
Application Dates	Valid: 08.06.2015	Expiry: 07.09.2015	Target: Over 13 weeks
Officer	Matthew Clapham		
Executive Summary	<p>This application for a new commercial building of 4,338sqm within a B1c (Light Industry), B2 (General Industry) and B8 (Storage or Distribution) use is located on a previously developed brownfield site located within a designated Employment Area and is acceptable in principle. The site adjoins the recently developed Costco site and formed part of the original Dairy Crest operation which was demolished to allow for the Costco development. This area of land was superfluous to Costco's needs. From an economic development point of view, the proposal could lead to additional employment and benefits for local people and local businesses.</p> <p>It is considered that the design of the proposal is acceptable within the street scene and surrounding area. The Environmental Health Officer considers that, subject to conditions, the noise from the proposed development will not adversely affect the nearest dwellings. In view of the fact that there is no noise objection and given the distance to the nearest dwellings, the proposal would not have an unacceptable impact on the amenity of the residential properties in terms of loss of light or outlook and in this respect the application is acceptable. The proposal is also acceptable in terms of flooding, contamination, renewable energy and air quality. In terms of transportation matters, Surrey County Council and Highways England are satisfied with the proposals subject to conditions. The Lead Local Flood Authority at Surrey County Council is satisfied with the drainage elements of the proposal subject to a suitable condition ensuring access to the existing Thames Water drainage system.</p>		
Recommended Decision	This application is recommended for approval.		



## MAIN REPORT

### 1. Development Plan

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

SP3 (Economy and Employment Land Provision)

EM1 (Employment Development)

LO1 (Flooding Implications of Development)

CO2 (Provision of Infrastructure for New Development)

SP6 (Maintaining and Improving the Environment)

EN1 (Design of New Development)

EN3 (Air Quality)

EN15 (Development on Land Affected by Contamination)

SP7 (Climate Change and Transport)

CC1 (Renewable Energy, Energy Conservation and Sustainable Construction)

CC2 (Sustainable Travel)

CC3 (Parking Provision)

### 2. Relevant Planning History

2.1 The site and adjoining land has a detailed planning history. However, the more relevant and recent applications are set out below:

13/01029/FUL	Demolition of existing buildings on site and the erection of a "warehouse club" (13,006sqm), including tyre installation, sales and associated facilities with access and associated parking and landscaping. (NB. Costco Application – Now constructed and open).	Grant Conditional 15.03.2014
SPE/FUL/86/118	Erection of a timber acoustic barrier fence (to address noise complaints from local residents to the rear)	Approved
SUN/FUL/1177Z	Erection of a single storey	Approved

warehouse accommodation of 1967  
some 262,000 sq. ft. in two units  
with ancillary accommodation of  
3,000 sq. ft. (N.B related to current  
application site plus adjoining  
land).

### 3. Description of Current Proposal

- 3.1 The application site comprises a rectangular plot situated on the northern side of Hanworth Road, accessed via the slip road between the Sunbury Cross Roundabout and the A316. On the eastern side is the recently constructed Costco and to the other is a secure storage facility (Lock'n'store). It is located within a designated Employment area.
- 3.2 The site was previously occupied by a commercial building used by Dairy Crest, which has been demolished to facilitate the construction of the Costco Warehouse Club located to one side of this application site. There was previously a footprint of about 6626sqm. of commercial development on the application site.
- 3.3 It is proposed to erect a new building to provide a development comprising various B1, B2 and B8 with a total floor space of 4,338sqm including four loading bays to the rear and 43 parking spaces to the front. B1 office accommodation is shown to be located on a first floor level to the front of the building. Overall there is a net difference in the floorspace of commercial development on the site of 2288sqm. on the application site. The applicant has confirmed that there is no confirmed occupier for the building at this stage, however the intention is that the building will provide high quality Head Quarter accommodation to a future occupier.
- 3.4 The proposal will utilise the existing left in, left out access to Costco which itself leads onto the slip road onto the A316.
- 3.5 The building is a rectangular building with a twin curved design to the roof with a maximum height to the apex of 12.5m The eaves / haunch level is 10m. The material schedule submitted with the application shows the building with be clad using grey, aluminium and silver coloured cladding.
- 3.6 A Transport Assessment, Energy Statement, Air Quality Assessment, Noise Assessment, Flood Risk Assessment and a Planning & Design Statement have been submitted with the application.
- 3.7 Copies of the site layout and proposed elevations are provided as an Appendix.

#### 4. Consultations

4.1 The following table shows those bodies consulted and their response.

Consultee	Comment
Highways England	No objections subject to conditions
County Highway Authority	No objections subject to conditions
The Environment Agency	No comments
Thames Water	No objections
London Borough of Hounslow	No objections
Surrey County Council (Lead Local Flood Authority)	Requested conditions
Environmental Health (Contamination)	No objections subject to condition
Environmental Health (Air Quality)	No objections subject to condition
Environmental Health (Noise)	Requested condition regarding acoustic fencing
Councils Sustainability Officer	No objections

#### 5. Public Consultation

42 neighbouring properties were notified of the planning application. No letters of objection have been received.

#### 6. Planning Issues

- Principle of commercial use / employment
- Design and appearance
- Noise
- Residential Amenity
- Transportation
- Air Quality
- Flooding/Drainage
- Contamination
- Renewable Energy
- Landscaping

#### 7. Planning Considerations

##### Employment development

7.1 The National Planning Framework (NPPF) states that “*the purpose of the planning system is to contribute to the achievement of sustainable development*”. In para. 7, the NPPF outlines three dimensions to sustainable: economic, social and environmental. The economic role involves “*contributing to building a strong, responsive and competitive economy, by ensuring that sufficient land of the right type is available in the*

*right places and at the right time to support growth and innovation; and by identifying and coordinating development requirements, including the provision of infrastructure...". Para 18 relates to the aim of building a "strong, competitive economy" and states that the "Government is committed to securing economic growth in order to create jobs and prosperity, building on the country's inherent strengths, and to meeting the twin challenges of global competition and of a low carbon future". Para 19 makes it clear that the "Government is committed to ensuring that the planning system does everything it can to support sustainable economic growth. Planning should operate to encourage and not act as an impediment to sustainable growth. Therefore significant weight should be placed on the need to support economic growth through the planning system."*

- 7.2 The site falls within an Employment Area (Hanworth Road and Country Way, Sunbury) in the Councils Core Strategy and Policies DPD, 2009 (CS&P DPD) and the Proposals Map DPD. Policy EMI states that the Council will: *retain such designated employment areas and support in principle proposals in these areas for employment development, encourage proposals for redevelopment and extensions that enable business need to be met and make the most effective use of available employment land, refuse proposals that make a net loss of employment land or floorspace in Employment Areas, unless the loss of floorspace forms part of a redevelopment that more effectively meets needs for an existing business operating from the site or the loss of employment land is part of a mixed use development on the site which results in no net loss of employment floorspace, or it can be clearly demonstrated that the maintenance of existing levels of employment floorspace on the site is unsustainable and unviable in the long term.*
- 7.3 The principle for a commercial development on this site has been established by the previous use on the site and additionally, it is designated within the Councils Core Strategy and Policies Development Plan Document (CS&P DPD) as an Employment Area.
- 7.4 The original proposal for the Costco development (to the east) and the demolition of the Dairy Crest buildings acknowledged that it would result in the loss of a warehouse (Class B8 use) but would be replaced by a wholesale "warehouse club" both of which are employment uses. The previous Dairy Crest buildings on the site amounted to 21,205 sqm of floorspace. This proposal would result in an additional floorspace of 4,338sqm. This should be considered in conjunction with the Costco development which created 13,006 sqm of floorspace. Therefore the total floorspace of this scheme and the Costco development is 17,344sqm which will lead to a cumulative loss of 3,861sqm of employment floorspace. The development will result in an improvement in the quality of the buildings and the appearance of the site as a whole and will provide valuable employment opportunities. As such, it is considered that the overall benefits of the scheme, providing a more modern, sustainable and viable employment use ensure that the proposal would not conflict with Policy EM1 of the CS&P DPD.

## Design and Appearance

- 7.5 Policy EN1 of the CS&P DPD requires a high standard in the design and layout of new development. The unit comprises a large storage / distribution area with ancillary offices at first floor level to the front of the unit. The design of the building and the indicative facing materials provide a modern and high quality style of development that would not be detrimental to the character and appearance of the local area. The proposed elevations and materials show some areas on the elevations with contrasting materials which are considered to 'break up' the appearance of the buildings and compensate for any functional aspects of the design.
- 7.6 The unit is located in a similar position on the site to the previous Dairy Crest building on this part of the site. In terms of height, the proposed building has a double sloping roof with a maximum height of 12.5m and the eaves being at 10m. This compares to the previous buildings on the site which was in excess of 40 years old and had a height of 6-9m and at a prominent location has no inherent design or visual merits. The adjoining Costco building is considerably higher at 14m. with a flat roof. Parking is located to the front of the unit with the service yard area to the rear.
- 7.7 The site is located off the A316, a busy dual carriageway which leads into London northbound and onto the M3 motorway southbound. It is located within a large commercial area bisected by the M3/A316 and with buildings of varying heights. Bearing this in mind, and given the existing screening to the north together with proposed new screening, it is considered that the design of the proposal is acceptable within the street scene and the surrounding area, including the residential properties to the rear and would accord with the requirements of policy EN1.

## Impact on Neighbouring Properties

- 7.8 Policy EN1 of the CS & P DPD states that new development should achieve a satisfactory relationship with adjoining properties avoiding significant harmful impact in terms of loss of privacy, daylight or sunlight, or overbearing effect due to bulk and proximity or outlook. It should be noted that there was previously a commercial use and building on the site and that the Costco warehouse has recently been constructed on part of the employment site. In view of this, the height of the building and the separation distances involved. The nearest gardens to residential properties are approximately 30m away to the rear / north, with the nearest dwellinghouses themselves being 35m away in Windsor Road. Therefore it is not considered that there would be any significant adverse impacts on neighbouring properties in terms of any loss of light, privacy or outlook.

## Noise

- 7.9 The Councils Environmental Health Department have reviewed the noise assessment and are satisfied that the erection of a 4m high acoustic fence to the rear of the site will be sufficient to minimise any noise impacts upon the adjoining residential properties arising from the service yard. The nearest properties being at least 35m away as outlined above. There is an existing 3.6m high fence to the rear boundary and the Costco development

required a 3m high acoustic fence to be installed. In view of the potential visual amenity issues of a 4m high fence, it is considered that a 3.6m high acoustic fence would be sufficient to overcome any significant noise concerns. A suitable condition can ensure suitable fencing is provided.

### Transportation

- 7.10 As explained in paragraphs 3.2 and 3.3 above, the proposal involves 2288sqm less floorspace on this site than previously existed. The site is part of a larger area on which the Costco proposal has now been completed. In assessing the net impact of this new development – which uses the same access as Costco – it is important to understand the implications of this additional floorspace and the extent to which the Costco scheme took into account in its traffic assessment of its scheme. The position is assessed as follows using sections of text and tables from Surrey County Councils consultation response.
- 7.11 The applicant submitted a Transport Statement with the application which detailed the relevant policies, existing and proposed transportation issues and details of projected trip generation and an assessment of potential highway impacts. Improvements were made to the Sunbury Cross Roundabout as part of the Costco application and vehicles accessing and egressing this site would utilise the existing left in, left out junction that allows access to Costco for both deliveries and customers and also the adjoining storage facility. The proposed uses of the site, including potential general industrial and warehouse / distribution uses have a lesser traffic generation characteristic than offices or retail or the particular operation of Costco. The proposed uses do not involve a ‘public’ facility. Careful consideration has to be given to highway safety and any potential conflicts between commercial lorries, staff cars and members of the public which all have to access and egress the site from the same access road and the same direction.
- 7.12 The County Highway Authority assesses that the developer is seeking a land use mix comprising 1,216m<sup>2</sup> of B1/B2 uses and 3,122m<sup>2</sup> of B8. The County Highway Authority believes a land use mix comprising at worst (in terms of traffic generation) 2000m<sup>2</sup> of B1/B2 use and 2622m<sup>2</sup> of B8 use could develop through changes of use that do not currently require planning permission. This could develop because permitted development allows changes of use from B8 to B1 up to a maximum, of 500m<sup>2</sup>. The most traffic intensive form of B1 use is an office which is reflected in trip rates agreed by the County Highway Authority. The forecast trip rates are likely to produce the movements shown in table 1 below.

Table 1

Land use	0800-0900		Total Movements	1700-1800		Total Movements
	Arrivals	Departures		AM	Arrivals	
B1 2000m <sup>2</sup>	33	3	36	3	25	28
B8 2622m <sup>2</sup>	11	5	16	2	8	10
Total movements	44	8	52	5	33	38

7.13 Table 2 below shows the vehicle movements that could have occurred in the previous 21,205m<sup>2</sup> of B8 land use that once occupied the site comprising Costco and the current application site.

Table 2

Land use	0800-0900		Total Movements	1700 -1800		Total Movements
	Arrivals	Departures	AM Peak	Arrivals	Departures	PM Peak
B8 21,205m <sup>2</sup>	88	39	127	18	61	79

7.14 The County Highway Authority comment that Costco, in their planning application numbered 13/01029/FUL did not remove the existing vehicle movements shown in table 2 from their modelling work of Sunbury Cross. In terms of guidance on modelling work, Costco would have been correct to remove those existing movements, but decided not to in order to produce a robust assessment of the impacts of their proposed development (in effect exaggerating the scale of traffic generation). The accepted mitigation measures were based on this 'robust' assessment.

7.15 In order to assess whether Costco have over mitigated the impact of their development, another exercise was carried out as shown in tables 3 and 4 below.

Table 3

Land use	0800-0900 Peak movements	1700 -1800 Peak movements
Former 21,205m <sup>2</sup> B8 use	127	79
Total movements	127	79

Table 4

Land use	0800-0900 Peak movements	1700 -1800 Peak Movements
Proposed 4622m <sup>2</sup> B1/B2 and B8	52	38
Costco	29	380
Total movements	81	418

7.16 Table 5 below shows the difference in vehicle movements between the former 21,205m<sup>2</sup> B8 (see table 3) land use had Costco have taken that into account and the combined traffic of the Costco site and the proposed development (see table 4).

Table 5

Land use	0800-0900 Peak	1700 -1800 Peak
Difference in vehicle movements	-46	+339

- 7.17 Table 5 shows the difference in vehicle movements after removing traffic from the former 21,205m<sup>2</sup> B8 use from the combined traffic of Costco and the proposed development. Costco provided mitigation for their 29 movements during the morning peak and 380 movements during the evening peak as shown in table 6. These movements are higher than the combined traffic of Costco and the proposed development minus traffic from the previous 21,205m<sup>2</sup> B8 use that once occupied the site which Costco and proposed development sits on. I am satisfied that traffic improvements have already been appropriately made to cover the additional traffic from the current proposal and the County Councils support for the scheme is soundly based.
- 7.18 In terms of parking the developer is proposing an adequate number of parking spaces by complying with Spelthorne Borough Council maximum parking standards. In terms of servicing the site, the developer is proposing four lorry bays. This is below the 'requirement' outlined in the Spelthorne Borough Council parking Standards which requires 23, based on 1 lorry space per 200 sqm of development. This standard takes no account of the scope for greater efficiency in the use of parking for larger developments or the range of operational arrangements Industrial / Warehouse operators may have. The standard is more applicable for large distribution operations with vehicles based at a site rather than storage / general industrial use and businesses who may make greater use of timed deliveries. Both the County Highway Authority and Highways England have not objected to the proposed development notwithstanding the shortfall in lorry loading bays. The spaces and service areas are set out adequately allowing entry and exit in forward gear.
- 7.19 In terms of bicycle parking the developer is proposing 10 spaces, but it is not clear whether they are to be under cover. Spelthorne Parking Standards require a minimum of 15 spaces for the proposed mix of development taking account changes of use. I have recommended a condition below to achieve the minimum number of bicycle parking spaces in a sheltered location.
- 7.20 The County Highway Authority have recommended a condition for the applicant to provide a travel statement to maximise in the use of public transport given the location of the site.
- 7.21 Highways England have also assessed the application and have not raised any objections to the proposal subject to a suitable condition. Therefore it is considered that the proposal is acceptable in terms of parking and highway safety.

#### Air Quality

- 7.22 The Councils Environmental Health Officer has reviewed the Air Quality Assessment submitted with the application. The previous use of the site was noted, as is the fact that the site was split to create two separate developments with the neighbouring Costco development site having already been developed and covers the larger portion of the original site. The improvement to the road network with the widening the northbound exit of the Sunbury Cross roundabout towards the A316 to two lanes and a financial contribution towards the Council's Action Planning measures have already ameliorated some air quality issues. In view of the small number of daily vehicles anticipated from this development (and the possibility that



there might be a reduction in traffic compared to existing use), it is not considered reasonable to request further mitigation measures from the redevelopment of the remaining portion of the original site. A condition requiring dust management to be in accordance with the recommendations of the Air Quality Assessment has been requested. On this basis, no objections are raised in terms of Air Quality.

#### Flooding and Drainage

- 7.23 Surrey County Council's Sustainable Drainage team in their role as Lead Local Flood Authority have reviewed the drainage details and subject to various conditions are satisfied that there are no specific flood concerns and that a suitable SuDs drainage system may be installed at the site. A condition is also required to ensure that the applicant gets the required permission from Thames Water in order to access their drainage network. The Environment Agency have judged the proposal as having low environmental risk and have not made any comments.

#### Contamination

- 7.24 The NPPF requires that decisions should ensure that:

*“the site is suitable for its new use taking account of ground conditions and land instability, including from natural hazards or former activities such as mining, pollution arising from previous uses and any proposals for mitigation including land remediation or impacts on the natural environment arising from that remediation; after remediation, as a minimum, land should not be capable of being determined as contaminated land under Part IIA of the Environmental Protection Act 1990; and adequate site investigation information, prepared by a competent person, is presented.”*

- 7.25 Policy EN15 of the CS&P DPD is concerned with development on land affected by contamination. The Council will expect proposals to be accompanied by an assessment of risk from contamination *“in all cases where the proposal is for housing or other forms of development which are particularly sensitive to contamination”* which are bound to be met.
- 7.26 The applicants have submitted a Geo-Environmental site investigation report and additional groundwater chemical analysis which have been assessed by the Council's Pollution Control Officer. The initial contamination assessment for the Costco development reviewed the site in its entirety, including this part of the site. However the remediation measures recommended in that assessment were only carried out on that part of the site covered by Costco. As such, the Councils Environmental Health team have recommended that the standard remediation condition is attached to this proposal to cover this part of the site.

#### Renewable Energy

- 7.27 The NPPF emphasises the importance of meeting the challenge of climate change and supporting the delivery of new and low carbon energy which it regards as essential to the dimensions of sustainable development. Policy CC1 of the CS&P DPD supports the provision of renewable energy, energy efficiency and promotes sustainable development. This will be by requiring

some larger developments, including new buildings which exceed 100 sqm. to:

- a) Optimise design, layout and orientation of development to minimise energy use
- b) Include measures to provide at least 10% of the development's energy demand from on-site renewable energy sources, and
- c) Encouraging measures including attaining high energy efficiency and minimum impact on the environment to at least Code for Sustainable Homes – 3 star or BREEAM “very good” standard, encouraging high standards of sustainable construction and encouraging appropriate freestanding renewable energy schemes.

7.28 The applicant has submitted an energy statement. The Councils Sustainability Officer is satisfied that the details contained in the Statement and subsequent additional calculations are satisfactory and meet the Councils requirements to ensure developments achieve 10% of their energy needs by way of on-site renewable energy sources.

#### Landscaping and Biodiversity

7.29 The NPPF requires the planning system to “*contribute to and enhance the natural environment*” by a number of ways including “*minimising impacts on biodiversity and providing net gains in biodiversity where possible. The advice also encourages “opportunities to incorporate biodiversity in and around developments...”*”

7.30 Policy EN8 of the CS&P DPD seeks to protect and improve the landscape and biodiversity through a number of means, including safeguarding sites of international and national importance, ensuring that new development avoids harm to nature biodiversity and refusing permission where development would have a significant harmful impact on nature conservation value.

7.31 The Phase 1 Habitat Survey submitted as part of the Costco development reviewed the site in its entirety including this part of the site. It identified that the majority of the site comprised areas of hard standing and a building. These were considered to be of low ecological value. Due to the size of the site, its previous use and the access road into Costco being located to the rear of the site, there is limited scope for substantial landscaping and planting. Previously for the Costco development, Natural England, Surrey Wildlife Trust and the Councils Tree Officer and Sustainability Officers did not raise any objections. A suitable condition requiring details of landscaping to be submitted is considered satisfactory in order to achieve some enhancement to the site when viewed from both the street scene and residential properties to the rear. It is not considered that the development would give rise to significant adverse effects. It is considered that the application is acceptable in ecological terms and complies with EN8 of the CS&P DPD and the NPPF.

## Other matters

- 7.32 In terms of lighting, a suitable condition is recommended requiring a detailed lighting scheme to ensure that appropriate lighting is provided on the site whilst without light spill beyond the site boundary. In view of the lighting on the adjoining Costco site which has a significant external parking area compared to this proposal, it is not considered that any lighting would have an unacceptable relationship with residential dwellings to the north. Highways England have raised no objection in terms of light spillage onto the A316.

## Conclusion

- 7.33 The proposed employment use on the site complies with the Councils policies being in a designated employment area. The siting and design of the proposed building is acceptable and would not be detrimental to the character and appearance of the area. Satisfactory parking, turning and access arrangements are to be provided which would ensure that there are no adverse impacts upon Highway Safety. It is considered that the application will have an acceptable impact on the amenity of neighbouring properties and is recommended for approval subject to conditions.

## **8. Recommendation**

- 8.1 GRANT subject to the following conditions:-

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: This condition is required by Section 91 of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) No development shall take place until full details of both soft and hard landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. The trees and shrubs shall be planted on the site within a period of 12 months from the date on which development hereby permitted is first commenced, or such longer period as may be approved by the Local Planning Authority, and that the planting so provided shall be maintained as approved for a period of 5 years, such maintenance to include the replacement in the current or next planting season whichever is the sooner, of any trees or shrubs that may die, are removed or become seriously damaged or diseased, with others of similar size and species, unless the Local Planning Authority gives written permission to any variation.

Reason: To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

- 3) No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a plan

indicating the positions, design, materials and type of boundary treatment including acoustic fencing to be erected in accordance with a timetable agreed in writing with the Local Planning Authority. Development shall be carried out in accordance with the approved details and maintained as approved.

Reason: To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

- 4) Before any work on the development hereby permitted is first commenced details of the materials and detailing to be used for the external surfaces of the buildings and surface material for parking areas be submitted to and approved by the Local Planning Authority.

Reason: To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

- 5) That within 3 months of the commencement of any part of the development permitted, or such longer period as may be approved by the Local Planning Authority, facilities shall be provided within the curtilage of the site for the storage of refuse and waste materials in accordance with details to be submitted to and approved by the Local Planning Authority before any work on the development hereby permitted is first commenced, and thereafter the approved facilities shall be maintained as approved.

Reason: To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009

- 6) No goods or articles shall be stored on any part of the application site except inside the buildings.

Reason: To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

- 7) Prior to the commencement of the development hereby permitted, details of the floodlighting of the car park and loading bays shall be submitted to and approved in writing by the Local Planning Authority, and the development shall be carried out in accordance with those details and thereafter maintained.

Reason: To safeguard the amenities of local residences in the vicinity.

- 8) The development shall be carried out in accordance with the dust management mitigation recommendations detailed in section 5.24 and Appendix 4 of the Air Quality Assessment produced by BWB Consultancy submitted 13 May 2015.

Reason: To protect local air quality within an air quality management area.

- 9) No development shall take place until:-

(i) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site, has been submitted to and approved in writing by the Local Planning Authority.

(ii) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.

(iii) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

Reason:- To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

#### NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination: Guidance to Help Developers Meet Planning Requirements" providing guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

- 10) Prior to the first use or occupation of the development, and on completion of the agreed contamination remediation works, a validation report that demonstrates the effectiveness of the remediation carried out shall be submitted to and agreed in writing by the Local Planning Authority.

Reason:- To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

#### NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination: Guidance to Help Developers Meet Planning Requirements" providing guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

- 11) No new development shall be occupied until space has been laid out within the site in accordance with the approved plans for cars to be parked and for the loading and unloading of vehicles and for vehicles to turn so that they may enter and leave the site in forward gear. The parking/turning area shall be used and retained exclusively for its designated purpose.

Reason: This condition is required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users.

- 12) Notwithstanding the proposed plans the development shall not be occupied unless and until space has been laid out within the site in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority to park a minimum of 15 bicycles in a secure and sheltered location. Thereafter the sheltered bicycle parking area shall be maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

- 13) Prior to the occupation of the development a Travel Statement shall be submitted for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide".

The approved Travel Statement shall be implemented upon first occupation and for each and every subsequent occupation of the development, thereafter maintain and develop the Travel Statement to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

- 14) The development hereby approved shall not begin until details of an agreement obtained from the local sewerage undertaker stating that the proposal can discharge into the surface water sewer has been submitted to and approved in writing by the Local Planning Authority. This agreement should state a rate of discharge rate into the surface water system has been agreed. The development shall thereafter be carried out in strict accordance with those details.

Reason: To ensure an acceptable Sustainable Drainage System and to comply with Policy LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document (2009) and the advice contained within the NPPF, NPPG and Non-Statutory Technical Standards for SuDS.

- 15) The development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. Those details shall include:
- a) Information about the design storm period and intensity (1 in 30 & 1 in 100 (+30% allowance for climate change), discharge rates and volumes (both pre and post development), temporary storage facilities, means of access for maintenance, the methods employed to delay and control surface water discharged from the site, and the measures taken to prevent flooding and pollution of the receiving groundwater and/or surface waters;
  - b) Any works required off-site to ensure adequate discharge of surface water without causing flooding or pollution (which should include refurbishment of existing culverts and headwalls or removal of unused culverts where relevant);
  - c) details of storage volumes for each attenuation Sustainable Drainage system feature are submitted

Reason: To ensure an acceptable Sustainable Drainage System and to comply with Policy LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document (2009) and the advice contained within the NPPF, NPPG and Non-Statutory Technical Standards for SuDS.

- 16) Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to

and approved by the Local Planning Authority to demonstrate that the Sustainable Urban Drainage System has been constructed as per the agreed scheme.

Reason: To ensure an acceptable Sustainable Drainage System and to comply with Policy LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document (2009) and the advice contained within the NPPF, NPPG and Non-Statutory Technical Standards for SuDS.

- 17) Before the commencement of the construction of the building hereby approved, details of how the Sustainable Drainage System will be protected and maintained during the construction of the development shall be submitted to and approved by the Local Planning Authority. The development shall thereafter be carried out in strict accordance with those approved details

Reason: To ensure that the construction works do not compromise the functioning of the agreed Sustainable Drainage System.

- 18) Before the commencement of the construction of the building hereby approved, details of how the Sustainable Drainage System will cater for system failure or exceedance events, both on and offsite, must be submitted to and approved by the local planning authority.

Reason: To ensure that the proposal has fully considered system failure

- 19) Before the commencement of the construction of the building hereby approved, details of how the Sustainable Drainage System shall be maintained and who shall own and maintain the drainage system, must be submitted to and approved by the local planning authority.

Reason: To ensure an acceptable Sustainable Drainage System and to comply with Policy LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document (2009) and the advice contained within the NPPF, NPPG and Non-Statutory Technical Standards for SuDS.

- 20) Prior to construction of the development hereby approved, a drainage layout detailing the location of SUDs elements, pipe diameters and their respective levels must be submitted to and approved by the local planning authority

Reason: To ensure the drainage design meets the technical standards

- 21) Notwithstanding the provisions of the Town and Country Planning General Permitted Development Order 1995 (or any Order revoking and re-enacting that Order), no extensions or outbuildings shall be erected to the warehouse development hereby permitted without the prior planning permission of the Local Planning Authority.



Reason: To safeguard the appearance of the locality/amenity of neighbouring residential properties in accordance with policies SP6, EN1 and CC3 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

- 22) All extract systems/plant/air handling equipment, in the proposed units, shall be installed and maintained so that noise or vibration cannot be heard or felt at or beyond the boundary of the nearest noise sensitive premises. The extract system shall be 5dB(A), below the existing back ground level.

Reason: To secure reduction in the level of noise emanating from the buildings on amenity grounds.

- 23) No development shall commence until a report has been submitted to and agreed by the Local Planning Authority which includes details and drawings demonstrating how 10% of the energy requirements generated by the development as a whole will be achieved utilising renewable energy methods and showing in detail the estimated sizing of each of the contributing technologies to the overall percentage. The detailed report shall identify how renewable energy, passive energy and efficiency measures will be generated and utilised for each of the proposed buildings to meet collectively the requirement for the scheme. The agreed measures shall be implemented with the construction of each building and thereafter retained and maintained to the satisfaction of the Local Planning Authority unless otherwise agreed in writing.

Reason:- To ensure that the development is sustainable and complies with Policy SP7 and CC1 of the Spelthorne Development Plan Core Strategy and Policies DPD.

- 24) Prior to the commencement of the works a Construction Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The plan should include details of wheel washing facilities to ensure that mud is not deposited on the slip road and M3 which would be considered a risk to the users.

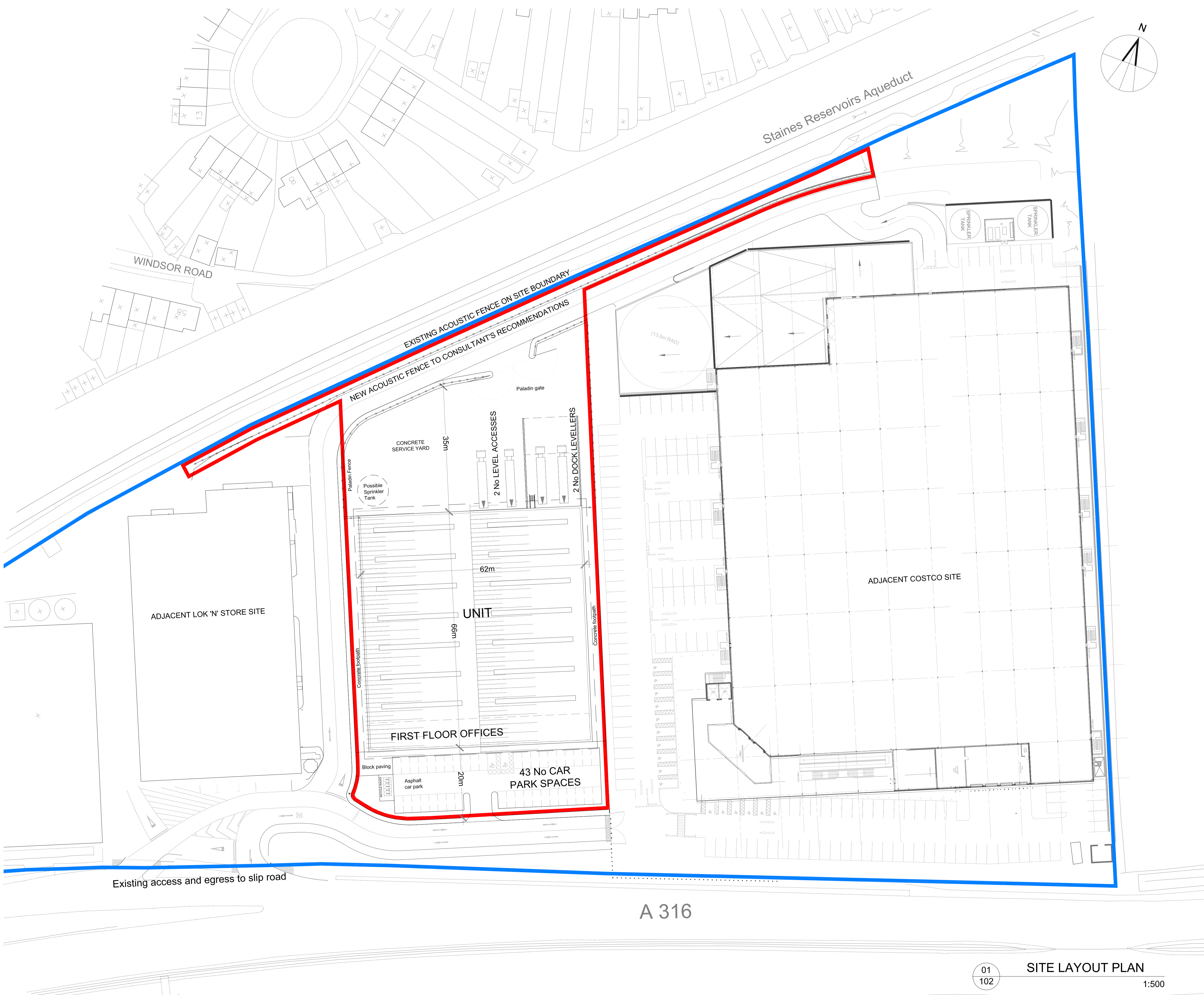
Reason:- To ensure the safety of motorists using the adjacent slip road and operation of the M3.

- 25) The development hereby approved shall be carried out in accordance with the following approved plans: 30076-PL-104A; 30076-PL-105A; 30076-PL-103; 30076-PL-101; 30076-PL-102 received 13 May 2015.

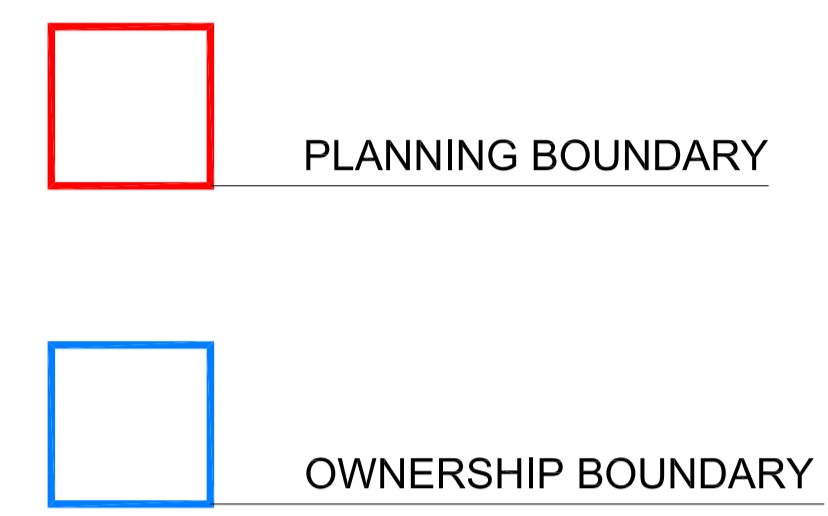
Reason:- For the avoidance of doubt and in the interest of proper planning.

## INFORMATIVES

1. We would recommend that the developer is referred to our advice note and evidence document on our website (<http://new.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice/more-about-flooding/suds-planning-advice>) for further guidance.
2. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
3. There is a Thames Water main crossing the development site which may/will need to be diverted at the Developer's cost, or necessitate amendments to the proposed development design so that the aforementioned main can be retained. Unrestricted access must be available at all times for maintenance and repair. Please contact Thames Water Developer Services, Contact Centre on Telephone No: 0800 009 3921 for further information.
4. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.



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<b>SITE AREA</b>	1.030	2.55



PLANNING				
REV	DATE	NOTE	DRAW	CHKD
/	03.03.2015	First Issue	SDD	DE

CHARTERED ARCHITECTS  
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LONDON SW5 4TU  
TELEPHONE 020 7735 6162  
FAX 020 7735 3896  
www.msa-architects.co.uk

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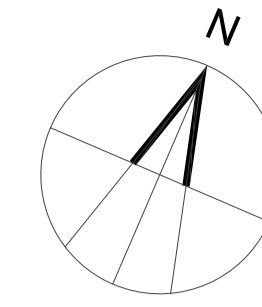
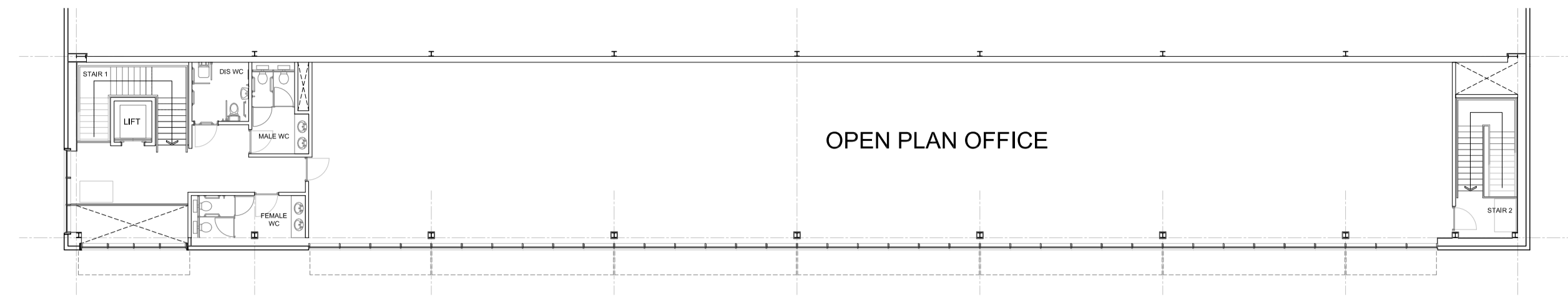
**DRAWING**  
SITE LAYOUT PLAN

**CLIENT**  
GRAFTONGATE

<b>DATE</b> MARCH 2015	<b>SCALE</b> 1:500 @ A1	<b>DRAWN</b> SDD
	<b>STATUS</b> PLANNING	<b>CHECKED</b> DE

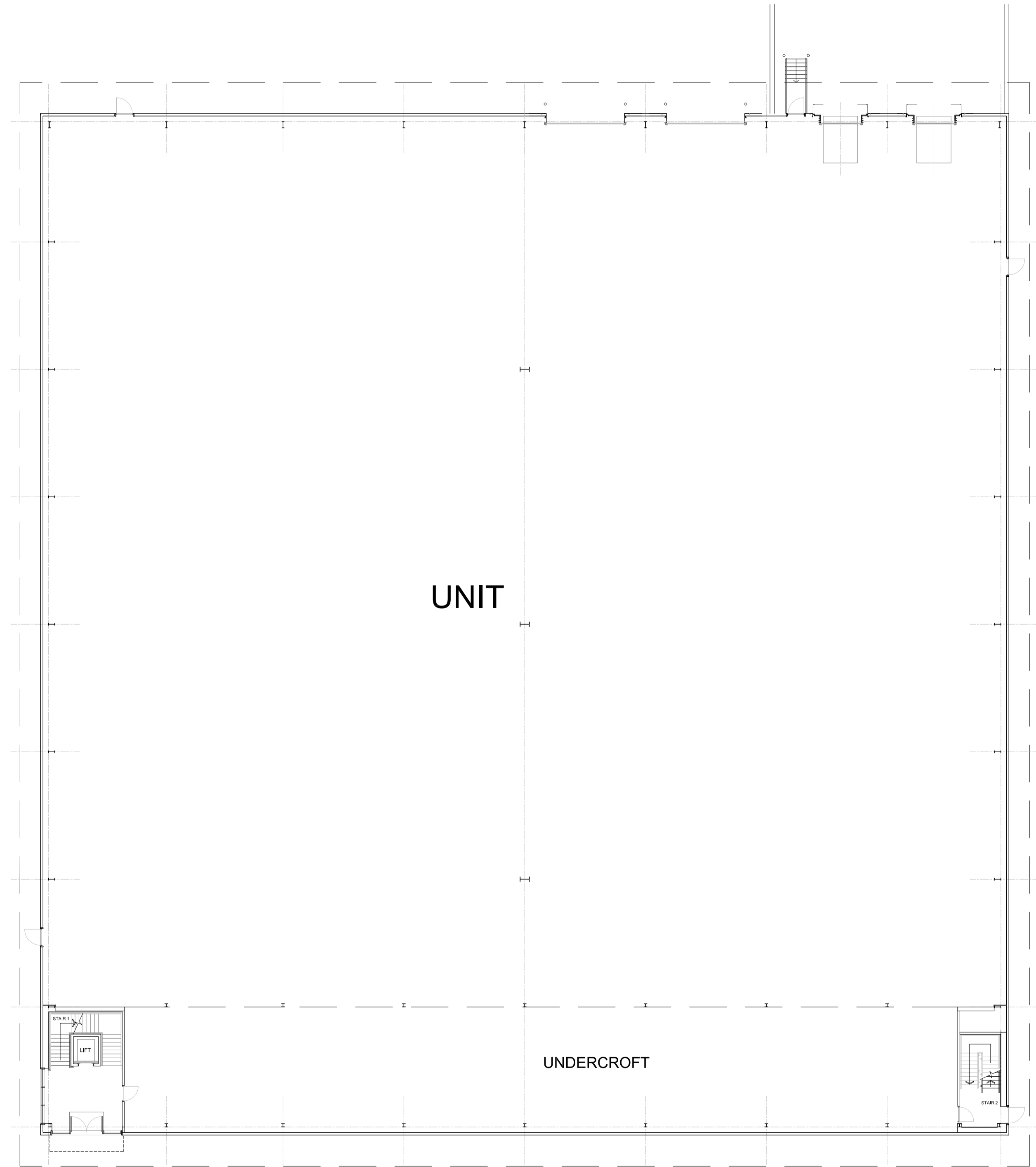
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02  
103  
FIRST FLOOR PLAN  
1:200



01  
103  
GROUND FLOOR PLAN  
1:200



PLANNING					
REV	DATE	NOTE	DRAW	CHK	
/	03.03.2015	First Issue	SDD	DE	



CHARTERED ARCHITECTS  
 11 PLATO PLACE  
 ST. DIONIS ROAD  
 LONDON SW6 4TU  
 TELEPHONE 020 7736 6162  
 FAX 020 7736 3896  
 www.msa-architects.co.uk





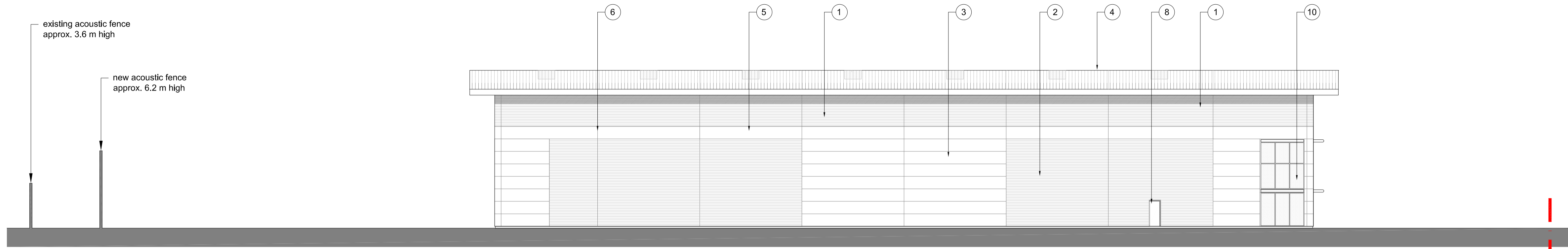
TITLE  
**HANWORTH RD, SUNBURY-ON-THAMES**

DRAWING  
**FLOOR PLANS**

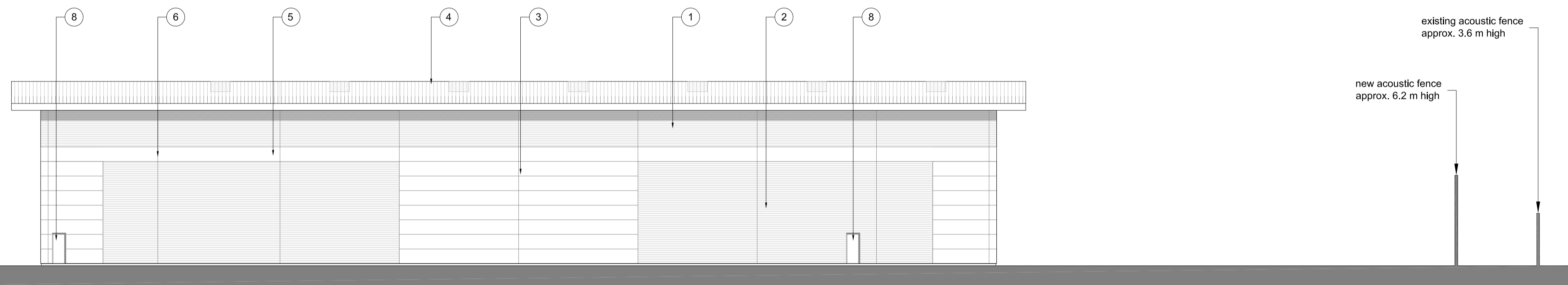
CLIENT  
**GRAFTONGATE**

DATE MARCH 2015	SCALE 1:200 @ A1	DRAWN SDD
STATUS PLANNING		CHECKED DE

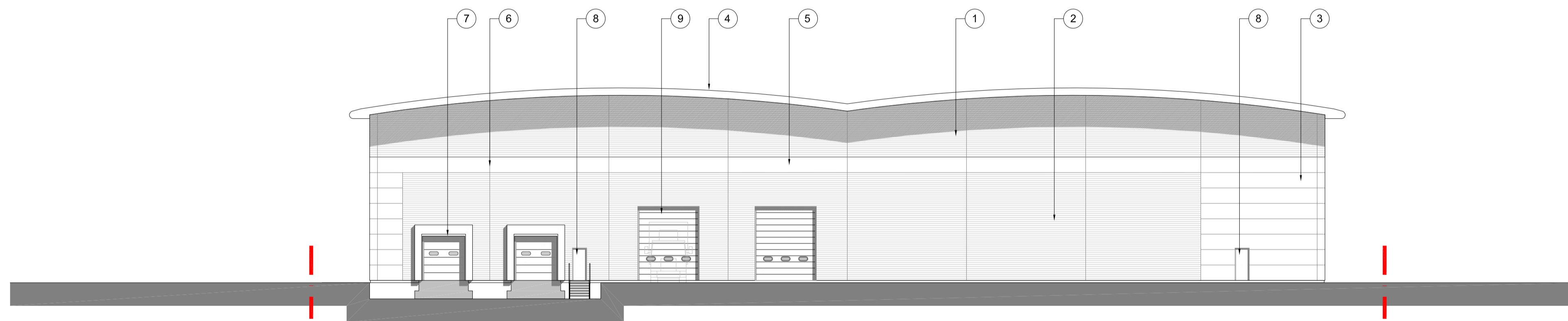
DRAWING NUMBER  
**30076-PL-103**



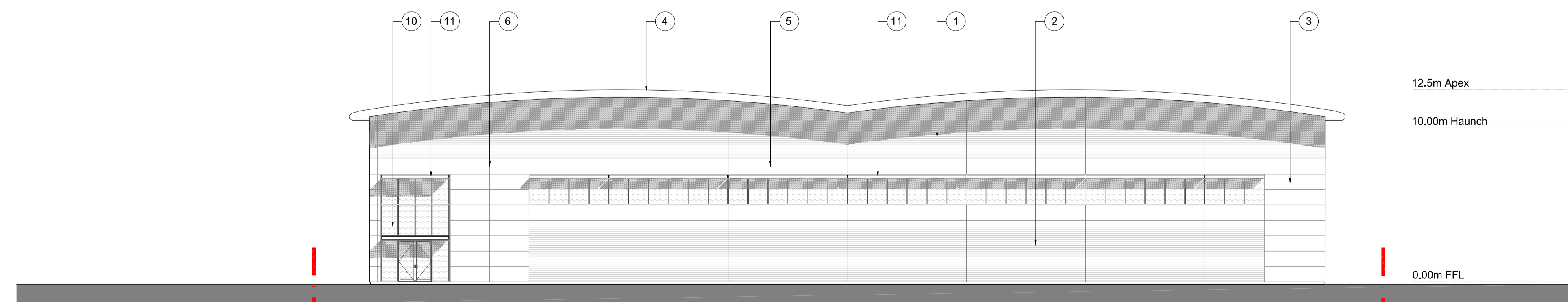
01  
104 WEST ELEVATION  
1:200



02  
104 EAST ELEVATION  
1:200



03  
104 NORTH ELEVATION  
1:200



04  
104 SOUTH ELEVATION  
1:200

12.5m Apex

10.00m Haunch

0.00m FFL

NOTES:

SUBJECT TO STATUTORY CONSENTS  
 SUBJECT TO SURVEY  
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MATERIAL SCHEDULE

- ① HORIZONTALLY LAID TRAPEZOIDAL PROFILED CLADDING GREY ALUMINIUM (RAL 9007)
- ② HORIZONTALLY LAID TRAPEZOIDAL PROFILED CLADDING GREY SILVER (RAL 9006)
- ③ KINGSPAN FLAT PANEL CLADDING COLOUR: METALLIC SILVER (RAL 9006)
- ④ ROOF PROFILED STEEL CLADDING FINISH: BRITISH STEEL COLORCOAT HPS200 COLOUR ALBATROSS (LIGHT GREY)
- ⑤ HIGH LEVEL HORIZONTAL FEATURE BAND FINISH: BRITISH STEEL COLORCOAT PRISMA COLOUR: COLOUR : METALLIC SILVER (RAL 9006)
- ⑥ T: FIN FLASHING COLOR GREY ALUMINIUM (RAL 9007)
- ⑦ DOCK LEVELLERS COLOUR: METALLIC SILVER (RAL 9006)
- ⑧ PERSONNEL DOORS COLOUR: METALLIC SILVER (RAL 9006)
- ⑨ LEVEL ACCESS LOADING DOORS COLOUR: METALLIC SILVER (RAL 9006)
- ⑩ CURTAIN WALLING DARK GREY FRAME (RAL 7016) / CLEAR GLASS
- ⑪ BRISE SOLEIL CANOPY CANTILEVERED BRISE SOLEIL CANOPY WITH BULL NOSE PROFILED FASCIA WITH CONCEALED FIXINGS. COLOUR: FINISH / METALLIC SILVER (RAL 9006)



PLANNING

REV	DATE	NOTE	DRAW	CHK
A	06.04.2015	Text alignment revised,	SDD	DE
/	03.03.2015	First Issue	SDD	DE



CHARTERED ARCHITECTS  
 11 PLATO PLACE  
 ST. DIONIS ROAD  
 LONDON SW6 4TU  
 TELEPHONE 020 7736 6162  
 FAX 020 7736 3896  
 www.msa-architects.co.uk



TITLE  
**HANWORTH RD, SUNBURY-ON-THAMES**  
 DRAWING  
**ELEVATIONS**

CLIENT  
**GRAFTONGATE**

DATE  
**MARCH 2015** SCALE  
**1:200 @ A1** DRAWN  
**SDD**

STATUS  
**PLANNING** CHECKED  
**DE**

DRAWING NUMBER  
**30076-PL-104A**

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15/01513/FUL

Headline House, Stanwell Road  
Ashford, TW15 3QH



1:1,250

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Planning Committee

13 January 2016



<b>Application No.</b>	15/01513/FUL		
<b>Site Address</b>	Headline House, Stanwell Road, Ashford		
<b>Proposal</b>	Erection of two storey building to provide 1 no. one bed flat, 6 no. two bed flats and 1 no three bed flat with associated parking and amenity space following demolition of the existing commercial building on site		
<b>Applicant</b>	Lumiere Properties Ltd		
<b>Ward</b>	Ashford Town		
<b>Call in details</b>	None		
<b>Case Officer</b>	Peter Brooks		
<b>Application Dates</b>	Valid: 25.11.15	Expiry: 20.01.16	Target: Under 8 weeks
<b>Executive Summary</b>	<p>The application site comprises a broadly rectangular plot of land located on the eastern side of Stanwell Road, on the junction with Chaucer Road. The site is currently occupied by a single storey commercial building with a mezzanine floor. The current structure covers the majority of the site with a small grass area on the Stanwell Road frontage and some off street parking/access on the Chaucer Road frontage. The wider area is characterised by two storey residential dwellings, with a mix of both detached and semi-detached. On the opposite side of the street to the west is St Hildas Church, a Grade II listed building.</p> <p>The proposal would involve the demolition of the existing building on site, and the construction of a single building which would comprise two stories with accommodation within the roofspace. The building would have staggered frontage onto Stanwell Road, and would 'turn the corner' into Chaucer Road. The flats would be served by a car parking area to the rear accessed from Chaucer Road, and a communal amenity area.</p>		
<b>Recommended Decision</b>	This application is recommended for approval subject to conditions.		



## **MAIN REPORT**

### **1 DEVELOPMENT PLAN**

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

- EN1 - Design of New Development
- EN5 – Buildings of Architectural and Historic Interest
- HO4 – Housing Size and Type
- CC3 - Parking Provision
- CC1 - Renewable Energy
- EN7 - Tree Protection

### **2 RELEVANT PLANNING HISTORY**

2.1 The current structure on site was constructed in the late 1980s, and prior to that the site was used as a garage. In recent times an application to redevelop the site for residential use was refused on design and layout grounds. The relevant applications are listed below:

14/01836/FUL

Erection of 2 no. two storey blocks containing a total of 4 no. one bed flats and 4 no. two bed flats (2 no. one bed and 2 no. two bed in each block) with associated parking and amenity space following demolition of existing industrial unit on site.

Refused 20.05.2015

SPW/FUL/87/5

Conversion of existing garage building to provide 422.4 sq m (4,547 sq ft) of office accommodation (involving the provision of a mezzanine floor in part), elevation alterations, alterations to Chaucer Road vehicular access and provision of 16 car parking spaces.

Grant Conditional 17.0.1987

### **3 DESCRIPTION OF CURRENT PROPOSAL**

3.1 The application site comprises a broadly rectangular plot of land located on the eastern side of Stanwell Road, on the junction with Chaucer Road. The site is currently occupied by a single storey commercial building with a mezzanine floor. The current structure covers the majority of the site with a small grass area on the Stanwell Road frontage and some off street parking/access on the Chaucer Road frontage. The wider area is characterised by two storey residential dwellings, with a mix of both detached and semi-detached. On the

opposite side of the street to the west is St Hildas Church, a Grade II listed building.

- 3.2 The proposal would involve the demolition of the existing building (footprint of 585m<sup>2</sup>) on site and the erection of a two storey building which would include habitable accommodation within the roofspace. The building has been designed to address both the Stanwell Road and Chaucer Road frontages. The Stanwell Road frontage would include staggered gable ends, which include glass balconies at first floor level. The gables would be staggered in such a way that on the corner of Stanwell Road and Chaucer Road the gable would be set back from the junction, and would allow the building to ‘turn the corner’ into Chaucer Road. The Chaucer Road frontage would again feature a gable end to help link the whole building together. The overall design, form and choice of materials would give the building a contemporary feel, albeit with a pitched roof and strong gable features. The building would have an area of flat roof, but this would be mostly obscured by the pitched roofs, and it is not considered it would be viewed from within any street aspect as a flat roof structure.
- 3.3 The building would be served by a parking area to the rear (east) of the building, with a total of 13 spaces, including 2 disabled bays. A communal amenity area would also be located to the rear of the building, and the front and sides of the site would be landscaped. An existing tree on the site on the Stanwell Road frontage would be retained. The building would incorporate an integral cycle store and bin store.
- 3.4 Directly opposite the site to the west lies St Hildas Church, a Grade II Listed Building. This church was completed in 1928, and is a prominent landmark within the area. Its design use of red brick is distinctive, and the proposal has reflected elements of the design and incorporated red brick.

#### **4 CONSULTATIONS**

- 4.1 The following table shows those bodies consulted and their response

<b>Consultee</b>	<b>Comment</b>
<b>County Highway Authority</b>	No objection subject to imposition of conditions
<b>The Council’s Tree Officer</b>	No objection subject to submitted AMS being adhered with
<b>Sustainability Officer</b>	No objection subject to renewable energy condition
<b>Councils Heritage Officer</b>	No objections
<b>The Council’s Head of Streetscene</b>	No objection, scheme should provide space for 3x660L waste and 3x660L recycling and 1x140L food bins
<b>Historic England</b>	Not relevant for comment
<b>Environmental Health - Pollution Control Officer</b>	No objection subject to imposition of

<b>(Contaminated Land)</b>	conditions
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## **5 PUBLIC CONSULTATION**

10 neighbour notification letters were sent, with 19 responses to date. These letters all objected along similar ground, which were

- lack of parking
- proposed building out of scale and character with the surrounding area
- general over development of the site
- harmful impact upon St Hildas Church.

## **6 PLANNING ISSUES**

- Design, Appearance and Visual Impact
- Residential Amenity
- Housing Size, Type and Density
- Parking
- Impact on Listed Building
- Impact on existing trees

## **7 PLANNING CONSIDERATIONS**

### Comment on reasons for refusal of previous application

7.1 The previous application (reference 14/01836/FUL) was refused on the 20 May 2015 for the following reasons:

*1. The proposed development is considered to represent an over development of the site, demonstrated by a cramped layout, parking to the front of the proposed structure and an irregularly shaped amenity area, resulting in an incongruous form of development within this locality. The proposal is therefore considered contrary to Policy EN1 of the Spelthorne Development Plan Core Strategy and Policies Development Plan Document and the Councils Supplementary Planning Document 2009 for the Design of Residential Extensions and New Residential Development 2011.*

*2. The proposal is considered, by virtue of its layout, to provide an unacceptable level of off street parking provision. It has not been satisfactorily demonstrated that the parking spaces fronting Stanwell Road could be entered and left in a forward gear, so would result in an overall shortfall of on site parking provision which is considered would lead to a greater level of on street parking in the vicinity. The proposal is therefore considered contrary to Policy CC3 of the Spelthorne Development Plan Core Strategy and Policies Development Plan Document and the Councils Supplementary Planning Guidance on Parking Standards.*

*3. It is considered the proposed design of the buildings, lacking any architectural merit, in close proximity to the adjacent Grade II listed building*

*(St Hilda's Church) would not sustain and enhance its setting. The proposal is therefore considered contrary to Policy EN5 of the Spelthorne Development.*

- 7.2 The previous refused scheme featured two identical blocks, and it was considered their design and layout would have led to site being unacceptably cramped. The scheme under consideration here comprises a single building, which has consequently allowed more space to be included around the building to be used for car parking, amenity space and landscaping areas.
- 7.3 The proposed parking layout has been rearranged, so that all the spaces are located to the rear of the proposed building. As this proposal proposes to include a different mix of bedroom flats to that previously refused (but still 8 in total) in order to meet the Council's minimum parking standard the applicant has had to provide 13 spaces. These would utilise the existing crossover onto Chaucer Road.
- 7.4 The scheme under consideration here has been designed to pick up on features of the adjacent Listed Building, whilst maintaining its own distinct identity. It is considered the staggered gables, balconies and choice of materials would not cause harm to the setting of the Listed Building, and would represent an improvement over the existing industrial looking building on site.

#### Design, Appearance and Visual Impact

- 7.5 The Proposed building would have a smaller footprint than the existing building. The existing building has a shallow pitch roof over, with a maximum height of 7.31m. The proposed building would have a maximum height of 9m. Whilst this is clearly higher than the existing, due to the significantly reduced footprint it is considered the increase in height would not have an unacceptable impact on the streetscene. The building's height would not appear out of context within both the Stanwell Road and Chaucer Road frontages, and would in no way 'compete' with the adjacent church in terms of scale. It is considered the scale of the building, when viewed in relation to the whole site, and its parking, amenity and landscaped areas would be acceptable, and it would not be viewed as an unacceptably large building on the plot, especially when compared to the footprint of the existing building. The proposal would respect the building line within both Stanwell Road and Chaucer Road. The element closest to no. 104 Stanwell Road (the adjoining detached dwelling to the north) would be in line with that property's frontage, before stepping out slightly to the first of three staggered gable frontages. The northern most gable would be set forward of the prevailing building line, and the other two gables would be set in line with no. 104 Stanwell Road and then further back respectively. The corner closest to no. 106 Stanwell Road (the adjoining dwelling to the south) would be set behind the front of this building. The Chaucer Road frontage would have a small element forward of the prevailing building line, but the majority of this frontage would be broadly in line with the properties on the northern side of the street.
- 7.6 The proposal would therefore be of a layout and form that would respect the character of the area, and would successfully address this prominent corner plot. The building would be of a comparable height to the adjoining dwellings.

Its contemporary design and choice of materials including red tiled roof that would help to reflect the red brick of the adjacent church, whilst also using more modern materials such as the glass balustrade would help the building maintain its own identity.

- 7.7 The proposal would include habitable accommodation within the roof. In order to provide sufficient light to these rooms, a number of rooflights have been included. It is considered the design of the roof would not appear out of scale with the host building, and the rooflights have been positioned symmetrically. The proposal would include an area of flat roof, and it is considered that this would not be so visually prominent as to cause harm to the wider area. It should be noted the existing building has two parapet roof edges on both flanks which give the impression of a flat roof, and the adjoining church hall building has a flat roof section. It is considered the flat roof element proposed would be an acceptable design form in this location.
- 7.8 The proposed shared amenity area would be located in the north east corner of the site, and be relatively well screened from the surrounding area so as to not feel unacceptably overlooked. The proposal would include a landscaped frontage along both Stanwell Road and Chaucer Road, and would retain the existing tree located at the front of the site.
- 7.9 The proposed parking area would be located to the rear (east) of the proposed building. It would include a total of 13 spaces, including 2 disabled. It is considered its position to the rear of the building would be acceptable and would not lead to the parking area having a dominant appearance on the site. The car park would be partially screened from the street by landscape planting, which can be controlled by condition.

#### Residential Amenity

- 7.10 The proposed building would have an acceptable impact upon the amenities of the adjoining neighbouring properties. The scheme would not have an overbearing impact and would not result in an unacceptable loss of light to the adjoining properties. The nearest existing residential property to the proposed building is no. 104 Stanwell Road, the detached property to the north. The existing building, whilst being lower, it located directly on the boundary of the site, whereas the proposed new building would be set in from the boundary by over 1m. There is also an access road which runs between the application site and no. 104 which serves a trio of garages. The proposal would not infringe either a vertical or horizontal 45 degree line when measured from the closest windows serving habitable rooms of the adjoining dwellings. The 25 degree line would not be infringed from any fronts or rears of adjoining dwellings. The 45 and 25 degree lines are used as a guide to ensure that new development does not block light reaching the windows serving habitable rooms.
- 7.11 The proposed building would utilise on the Stanwell Road frontage elongated windows at ground and first floor level. The first floor would also feature balconies. These windows would allow the maximum amount of natural light into habitable rooms and provide an outlook to the adjacent church. There would be no window openings on the northern flank elevation closest to no. 104 Stanwell Road which could therefore give rise to unacceptable overlooking. The rear elevation would feature ground and first floor elongated windows, and

the first floor rear units would also be served by balconies which would overlook the shared amenity area. It is considered these rear facing windows and balconies would not lead to an unacceptable loss privacy to the adjoining dwellings due to their separation. The Councils residential design SPD requires a minimum 'back to flank' separation distance of 13.5m, and the proposal would have a separation of over 17m at its closest point. It is acknowledged that the proposal would include rear facing rooflights they would not constitute a third storey in terms of assessing overlooking, and they would not lead to unacceptable overlooking due to their skyward orientation. It is considered the window openings on the southernmost projecting elevation could give rise to an unacceptable level of overlooking to the rear patio area of no. 106 Stanwell Road. As such it is considered reasonable to recommend a condition to ensure that these windows are obscurely glazed to prevent overlooking. It is considered the rooflights due to their angle would not lead to direct overlooking of the adjoining dwellings. It is considered the proposed building would not have an overbearing impact on the adjoining properties due to its separation from them.

- 7.12 Turning to the amenity of the proposed occupiers of the eight flats, it is considered they would provide an acceptable level of residential amenity. The Councils residential design SPD sets out minimum size standards for new residential units, which each unit would comply with or exceed. The flats would be well provisioned with windows to allow natural light, and those rooms within the roofspace would be served by rooflights. The scheme would require a minimum of 205m<sup>2</sup> of amenity area to fulfil the requirement as set out in the residential design SPD (35m<sup>2</sup> for the first five units and then 10m<sup>2</sup> for each additional unit :  $35 \times 5 + 3 \times 10 = 205$ ). The proposal would provide over the minimum 205m<sup>2</sup> of amenity space including balconies (180m<sup>2</sup> of shared amenity space, 60m<sup>2</sup> of balconies).

#### Housing Size, Type and Density

- 7.13 The proposal would include 7 out of the 8 total units as 1 or 2 bed units and so would comply with Policy HO4 which requires new development of over four units to provide at least 80% of new units as one or two bedroom.
- 7.14 The proposed site would have a density unchanged from the previous scheme of 85 dwellings per hectare (DPH). This figure is above the usual guidelines (generally in the range of 35 to 55 DPH) for dwellings in this vicinity, however this high mathematical density is a product of 7 of the units being 1/2 bed flats. It should be noted also that the development has less site coverage than the existing development.

#### Parking

- 7.15 The proposal would provide a total number of 13 off street parking spaces which would be located to the rear of the building. This would comply with the minimum parking requirement for a development of this size as required by the Councils Parking Standards SPG (1.25 spaces per one bed unit, 1.5 spaces per two bed unit, 2 spaces per 3 bed unit: -  $1.25 \times 1 + 1.5 \times 6 + 2 \times 1 = 12.25$ ). The SPG explains that if the total parking figure is a fraction then the total must be rounded up to the nearest whole number. As such the proposal would be required to provide 13 spaces, which it has done, including 2 bays suitable for

the disabled. The proposal would provide a total of 20 secure cycle parking spaces which would more than comply with the required 1 cycle space per flat. These would be located within the main building.

- 7.16 It is acknowledged that there is within both Stanwell Road and Chaucer Road a large amount of on street parking, and a number of representations from neighbours raise this as an issue. The proposal however would comply with the Councils parking standards and therefore should not add to the problem.

#### Impact on the adjacent Listed Building

- 7.17 Section 66 of the Listed Building Act 1990 requires a planning authority to have 'special regard' to the desirability of preserving a listed building and its setting. The application site is opposite the Grade II listed building St Hilda's Church. St Hilda's church is a red bricked building completed in 1928, and features high gable ends. Any design should demonstrate that it would preserve the building or its setting. The proposed building has been designed with staggered gable frontages to add both visual interest to the building and to reflect the large gable end of this part of the listed church facing the application site and in ancillary elements of that building. The use of strong gable features and of red brick within parts of the proposed frontage would pay regard to those design features and materials within the church, and the modern materials such as the glass balustrades would contrast and allow the new building to maintain its own identity. The National Planning Policy Framework (NPPF) states that any harm caused to a Grade II listed building should only occur in exceptional circumstances; and it is not considered the proposal would cause harm to the listed building. Rather it is considered the proposal would serve to significantly improve the setting of the listed building by the removal of a 1980s factory building with no comparable design merit, with a sympathetic design which would enhance the setting of St Hilda's church.

- 7.18 The Councils Heritage and Conservation Area Officer has raised no objections to the proposal. He stated "*This application is a vast improvement in that it is respectful of and relates to the listed red brick church of St. Hilda. It is a crisp modern design. This with the use of glass balcony fronts should produce a subtle contrast with the surrounding architectural styles. The scale and massing broken by the use of gables, together with the horizontal and vertical articulation combine to make this an acceptable contribution to the street scene. The parking is now rationalised with a safe egress onto Chaucer Road, this leaves more meaningful areas for soft landscaping and amenity. Having had special regard to its effect on the listed St. Hildas church I consider that its setting would not be harmed*".

#### Renewable energy

- 7.19 The site can produce at least 10% of the developments energy need from on-site renewable energy sources, and this can be ensured by suitable planning condition.

### Impact on existing trees

- 7.20 The Council's tree officer has raised no objections to the proposal and its impact upon the existing trees on site subject to the imposition of a condition to ensure the trees are not damaged during construction. The proposed planting can be agreed by the imposition of a suitable planning condition.

### Conclusion

- 7.21 The proposal has overcome the reasons for refusal which related to the previous scheme on the site (reference 14/01836/FUL). The proposed alteration in the design, layout and use of materials would have an acceptable impact on the character of the area and the adjacent Grade II listed building. The alteration to the layout to include car parking wholly at the rear of the site, with a shared amenity area and landscaping would be a significant improvement over the previous scheme. The proposal would provide sufficient on-site car parking to meet the Council's standards, and the site's location close to Ashford town would be considered sustainable, and would encourage non-car based travel.

## **8 RECOMMENDATION**

- 8.1 The application is recommended for approval subject to the following conditions:

### **CONDITIONS**

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:-. This condition is required by Section 91 of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before any work on the development hereby permitted is first commenced details of the materials and detailing to be used for the external surfaces of the building(s) and surface material for parking areas be submitted to and approved by the Local Planning Authority.

Reason:-. To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

3. No development shall take place until:-  
(i) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site, has been submitted to and approved in writing by the Local Planning Authority.  
(ii) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent



of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.

(iii) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

Reason:-. To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination:Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

4. Prior to the first use or occupation of the development, and on completion of the agreed contamination remediation works, a validation report that demonstrates the effectiveness of the remediation carried out shall be submitted to and agreed in writing by the Local Planning Authority.

Reason:-. To protect the amenities of future residents and the environment from the effects of potentially harmful substances in accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination:Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

5. No development shall commence until a Construction Transport Management Plan, to include details of:
  - (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (e) provision of boundary hoarding behind any visibility zones
  - (h) measures to prevent the deposit of materials on the highway.

Reason:-. The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 (highway Safety) and CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

6. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking areas shall be retained and maintained for their designated purposes.

Reason:-. The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 (highway Safety) and CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

7. The development hereby approved shall not be first occupied unless and until the existing vehicular access to Chaucer Road has been modified in accordance with the approved plan, and redundant sections of the existing access have been reinstated to kerb and footway, all to be permanently retained.

Reason:-. The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 (highway Safety) and CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

8. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority for the storage of a minimum of eight bikes in a secure, covered and accessible location. Thereafter the bike storage area shall be retained and maintained for its designated purpose.

Reason:-. The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC3 (Parking) of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

9. That within 3 months of the commencement of any part of the development permitted, or such longer period as may be approved by the Local Planning Authority, facilities shall be provided within the curtilage of the site for the storage of refuse and waste materials (3 no. 660 litre rubbish bins, 3 no. 660

litre recycling bins, 1 no. 140 litre food waste bin), and thereafter shall be maintained as approved.

Reason:-.To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

10. Prior to the occupation of the development hereby permitted the first and second floor windows on the southern elevation located above the bin/cycle store shall be obscure glazed and be non-opening in accordance with details/samples of the type of glazing pattern to be submitted to and approved in writing by the Local Planning Authority. These windows shall thereafter be permanently retained as installed.

Reason:-. To safeguard the privacy of the adjoining property(ies), in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

11. No development shall commence until a report has been submitted to and agreed by the Local Planning Authority which includes details and drawings demonstrating how 10% of the energy requirements generated by the development as a whole will be achieved utilising renewable energy methods and showing in detail the estimated sizing of each of the contributing technologies to the overall percentage. The detailed report shall identify how renewable energy, passive energy and efficiency measures will be generated and utilised for each of the proposed buildings to meet collectively the requirement for the scheme. The agreed measures shall be implemented with the construction of each building and thereafter retained and maintained to the satisfaction of the Local Planning Authority unless otherwise agreed in writing.

Reason:-. To ensure that the development is sustainable and complies with Policy SP7 and CC1 of the Spelthorne Development Plan Core Strategy and Policies DPD.

12. No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a plan indicating the positions, design, materials and type of boundary treatment to be erected. The boundary treatment shall be completed before the building is occupied. Development shall be carried out in accordance with the approved details and maintained as approved.

Reason:-. To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

13. No development shall take place until full details of both soft and hard landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. The trees and shrubs shall be planted on the site within a period of 12 months from the date on which development hereby permitted is first commenced, or such longer period as may be approved by the Local Planning Authority, and

that the planting so provided shall be maintained as approved for a period of 5 years, such maintenance to include the replacement in the current or next planting season whichever is the sooner, of any trees or shrubs that may die, are removed or become seriously damaged or diseased, with others of similar size and species, unless the Local Planning Authority gives written permission to any variation.

Reason:-. To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development. In accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

14. (a) No demolition, site clearance or building operations shall commence until protective fencing consisting of weld mesh panels on a scaffold framework as shown in Figure 2 of BS5837 2012 has been erected around each tree or tree group to be retained on the site in accordance with details to be submitted to and approved by the Local Planning Authority before any work on the development hereby permitted is first commenced, such details to include trenches, pipe runs for services and drains. Such fencing shall be maintained during the course of the development and no storage of materials or erection of buildings shall take place within the fenced area.
- (b) The destruction by burning of materials shall not take place within 6 m (19 ft 8 ins) of the canopy of any tree or tree group to be retained on the site or on land adjoining.

Reason:-. To prevent damage to the trees in the interest of the visual amenities of the area, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

15. The measures set out in the Arboricultural report (ref: APA/AP/2015/181) be adhered to at all times, and any variation be submitted to and agreed in writing by the Local Planning Authority.

Reason:-. To prevent damage to the trees in the interest of the visual amenities of the area, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

16. The proposed hardstanding area shown on the submitted plan shall be constructed to be permeable, or be designed to run off to a permeable surface within the site, in accordance with details to be submitted to, and approved in writing by the Local Planning Authority before the commencement of the development. The hardstanding area shall be completed and maintained in accordance with the approved details.

Reason:-.To minimise the risk of flooding from surface water run off.

17. The development hereby permitted shall be carried out in accordance with the following approved plans HH-existing-fp-01a, HH-ELE-EX-01, HH-ELE-EX-02,HH-BP-02 all received 10.11.2015. 1537-PL/01A,1537-PL1/02A,1537-

PL1/03A all received 18.11.2015. 1537-PL1/04A, 1537-PL1/05A, 1537-PL1/06A and 1537-PL1/07A received 23.12.2015

Reason:-. For the avoidance of doubt and in the interest of proper planning.

18. No demolition, site clearance or building operations shall commence until a Demolition and Construction Method Statement, demonstrating that the works will not adversely affect the occupiers of neighbouring residential properties, has been submitted to, and approved in writing by, the local planning authority. The statement shall include measures to mitigate the impact of dust, noise and vibration. The statement shall include (unless otherwise agreed in writing by the Local Planning Authority):
- Provision of a 1.8m continuous close boarded fence around the site to be developed prior to works commencing;
  - Noise mitigation
  - Demolition details
  - Working hours to be:  
08:00 - 18:00 Monday to Friday  
08:00 - 13:00 on Saturdays  
No working on Sundays, Bank and Public Holidays.

Work shall proceed strictly in accordance with the approved Statement throughout the period of demolition, site clearance and construction..

Reason:-.In the interest of residential amenity.

### **INFORMATIVES TO APPLICANT**

- 1 The permission hereby granted shall not be construed as authority to carry out works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a licence must be obtained from the Highway Authority Local Transportation Service before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see <http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice/ordinary-watercourse-consents>.
- 2 The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
- 3 The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damages the highway from unclean wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway

surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

- 4 The applicant is advised that the site to which this planning permission relates is located on or near land that may contain harmful substances. Under Part C of the Building Regulations you will be required to consider this when designing the foundations of the development.

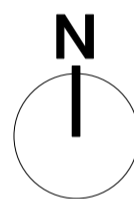
The applicant is advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences.

- 5 The applicant's attention is drawn to the requirements of the Party Wall Etc. Act 1996 in relation to work close to a neighbour's building/boundary.

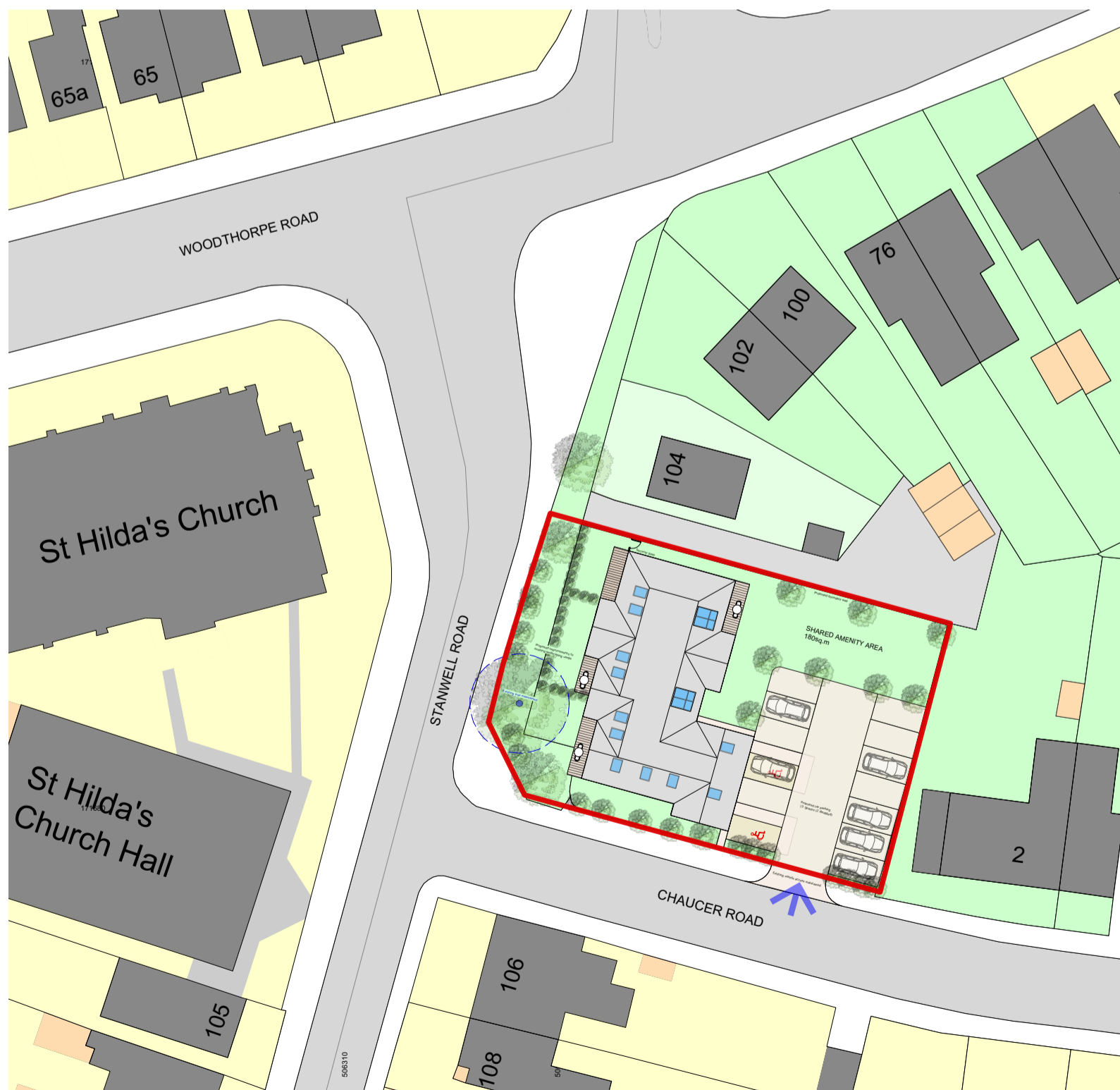
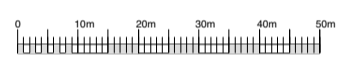




SITE LOCATION PLAN

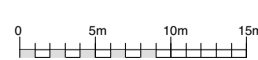


SCALE 1:1250 @ A2



BLOCK PLAN

SCALE 1:500 @ A2



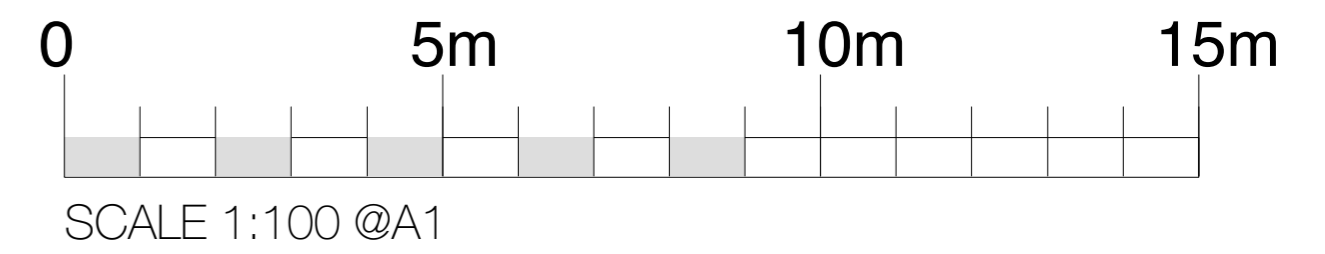
## PLANNING

REV.	DATE	DESCRIPTION
PROJECT		
PROPOSED REDEVELOPMENT HEADLINE HOUSE STANWELL ROAD ASHFORD, TW15 3QH		
DRAWING		
SITE LOCATION PLAN & BLOCK PLAN		
CLIENT		
LUMIERE PROPERTIES		
SCALE	1:1250 & 1:500 @ A2	DRAWN
DATE	OCTOBER 2015	CHECK
DRAWING No.	1537- PL1 /01	REVISION
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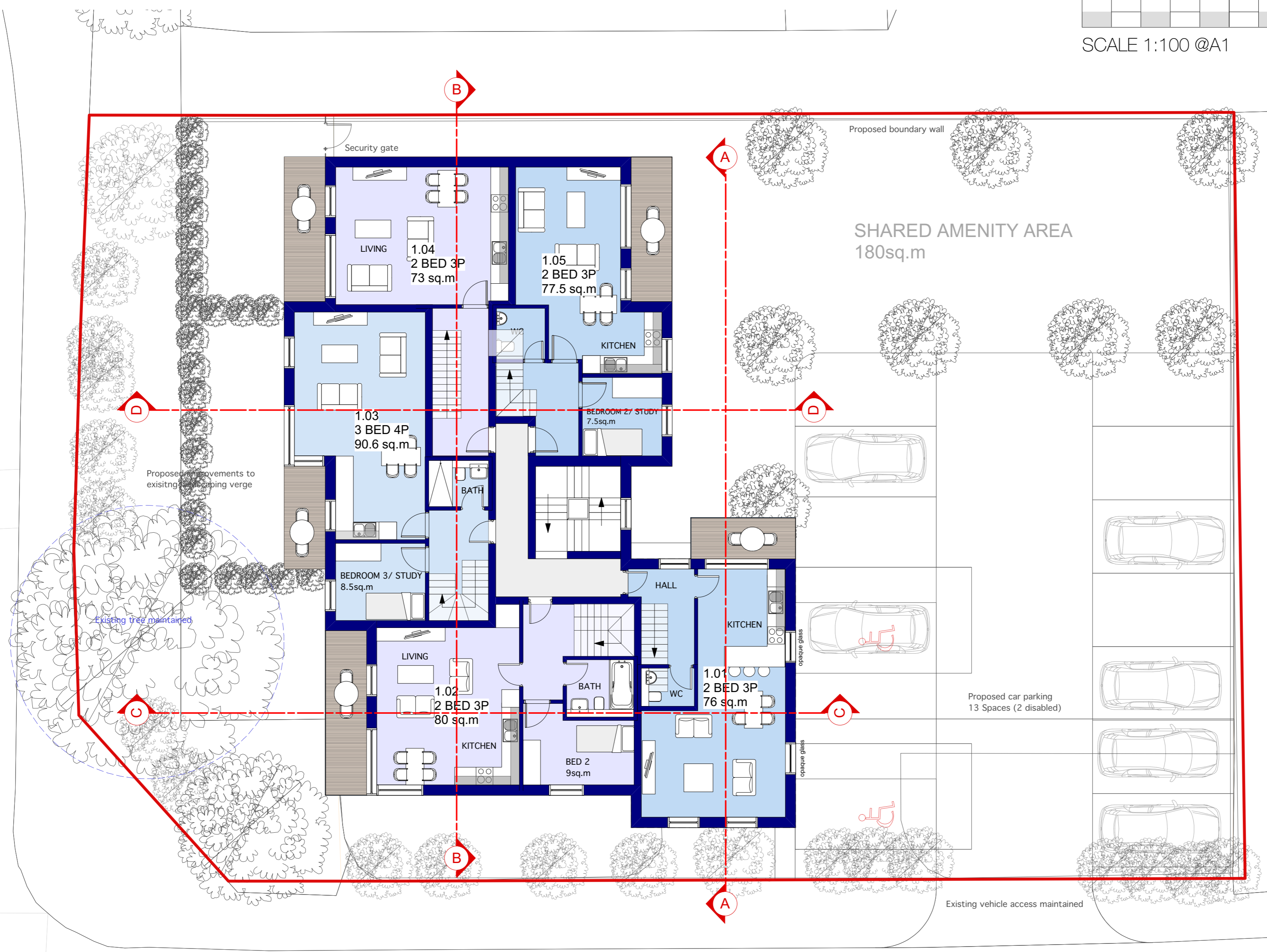
**LDA**  
Chartered Architects

LDA (Architects) Limited  
Lion House  
Oriental Road  
Woking  
Surrey. GU22 8AP  
T. 01932 828933  
E. lida@ldaarchitects.co.uk



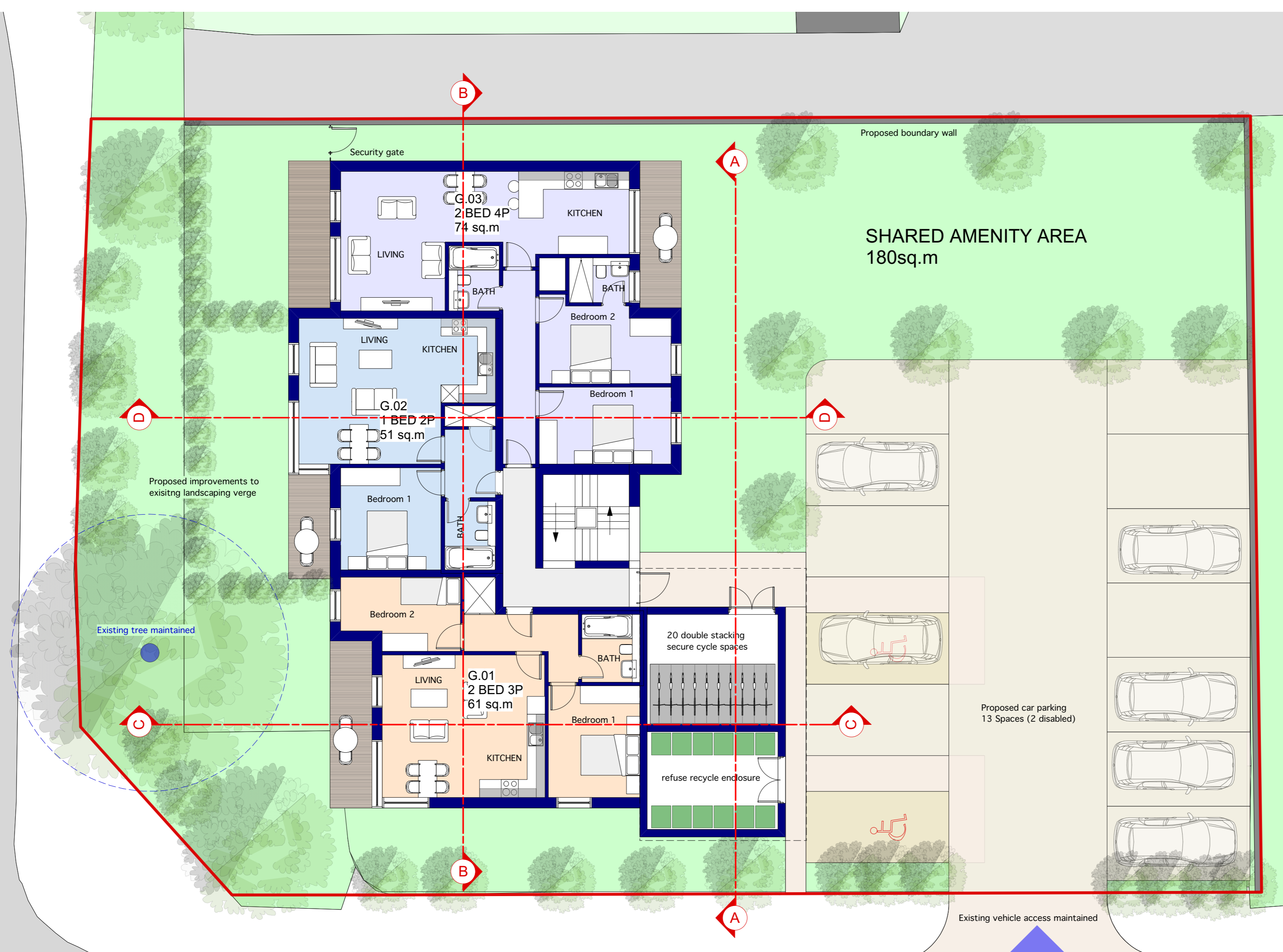


STANWELL ROAD



PROPOSED FIRST FLOOR PLAN

STANWELL ROAD



PROPOSED GROUND FLOOR PLAN

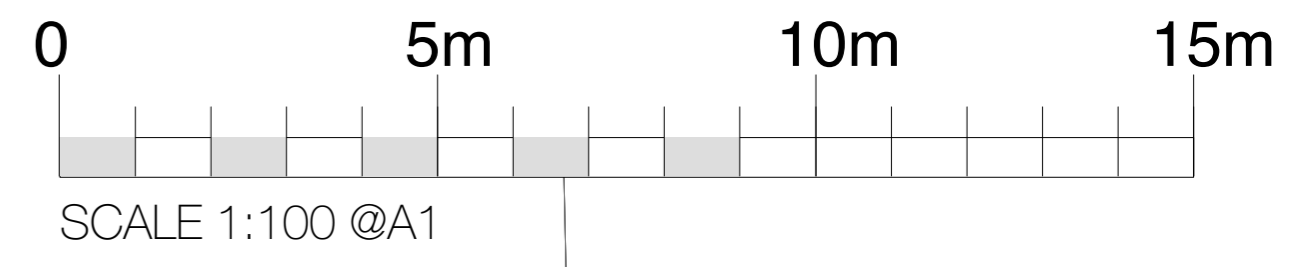
PLANNING

REV.	DATE	DESCRIPTION
PROJECT		
PROPOSED REDEVELOPMENT HEADLINE HOUSE STANWELL ROAD ASHFORD, TW15 3QH		
DRAWING		
PROPOSED GROUND & FIRST FLOOR PLANS		
CLIENT		
LUMIERE PROPERTIES		
SCALE	1:100 @ A1	DRAWN
DATE	OCTOBER 2015	CHECK
DRAWING No.	1537- PL1 /02	REVISION
		A
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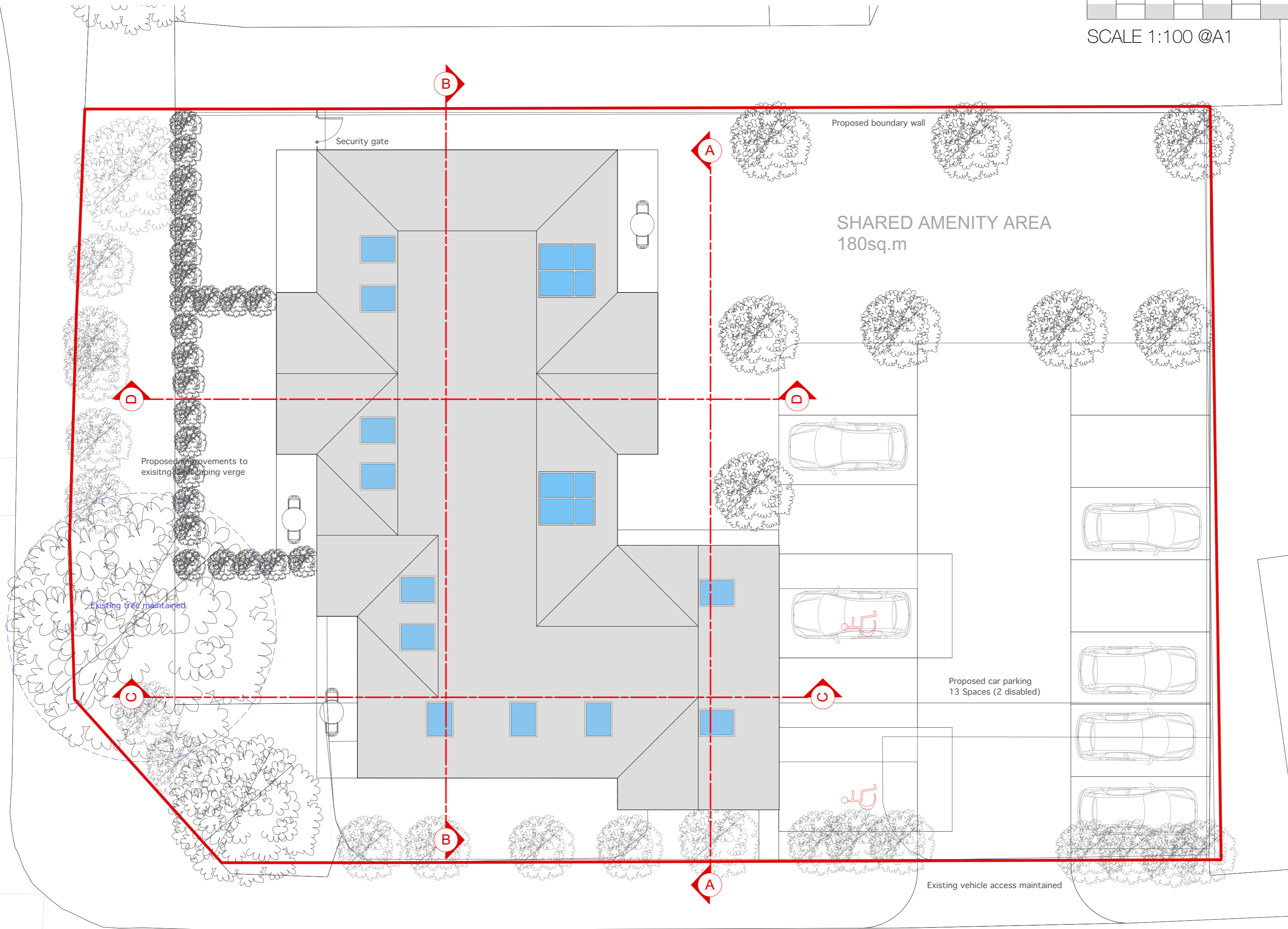
LDA  
Chartered Architects

LDA (Architects ) Limited  
Lion House  
Oriental Road  
Woking  
Surrey, GU22 8AP  
T. 01932 828933  
E. lida@ldaarchitects.co.uk

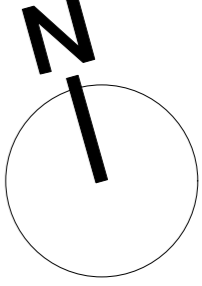




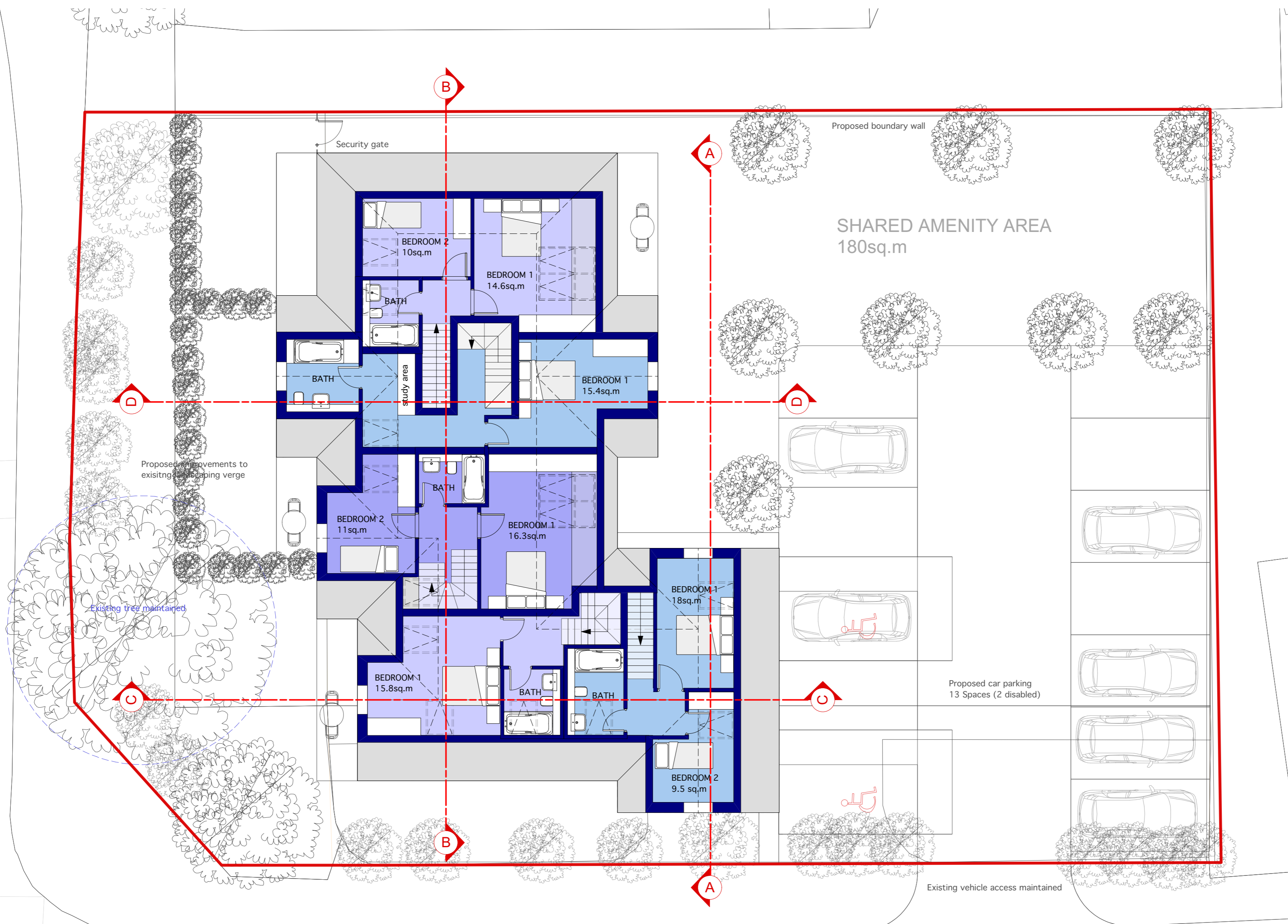
STANWELL ROAD



PROPOSED ROOF PLAN



STANWELL ROAD



PROPOSED SECOND FLOOR / EAVES PLAN

PLANNING

REV.	DATE	DESCRIPTION
PROJECT PROPOSED REDEVELOPMENT HEADLINE HOUSE STANWELL ROAD ASHFORD, TW15 3QH		
DRAWING PROPOSED SECOND/EAVES FLOOR & ROOF PLANS		
CLIENT LUMIERE PROPERTIES		
SCALE 1:100 @ A1	DRAWN	
DATE OCTOBER 2015	CHECK	
DRAWING No. 1537- PL1 /03	REVISION A	
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**LDA**  
Chartered Architects

LDA (Architects) Limited  
Lion House  
Oriental Road  
Woking  
Surrey, GU22 8AP  
T. 01932 828933  
E. lda@ldaarchitects.co.uk





PROPOSED WEST ELEVATION (STANWELL ROAD)



PROPOSED SOUTH ELEVATION (CHAUCER ROAD)



PROPOSED EAST ELEVATION



PROPOSED NORTH ELEVATION

PLANNING

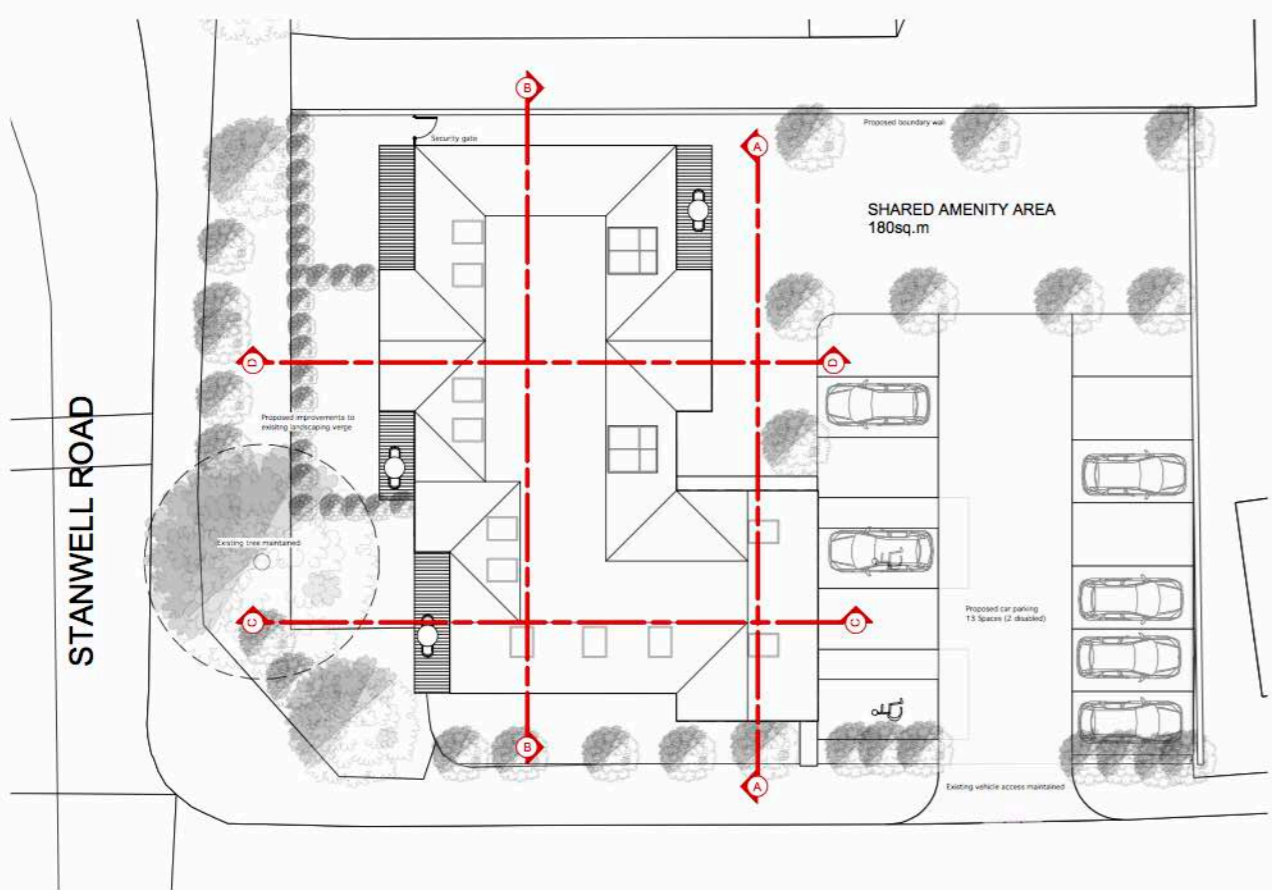
REV.	DATE	DESCRIPTION
PROJECT		
PROPOSED REDEVELOPMENT		
HEADLINE HOUSE		
STANWELL ROAD		
ASHFORD, TW15 3QH		
DRAWING		
PROPOSED ELEVATIONS		
CLIENT		
LUMIERE PROPERTIES		
SCALE	1:100 @ A1	DRAWN
DATE	OCTOBER 2015	CHECK
DRAWING No.	1537- PL1 /04	REVISION
		A
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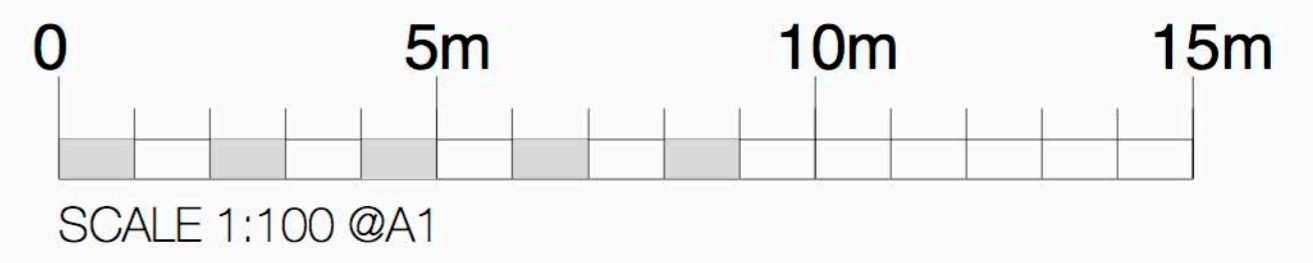
LDA (Architects) Limited  
Lion House  
Oriental Road  
Woking  
Surrey, GU22 8AP

T. 01932 828933  
E. lda@ldaarchitects.co.uk

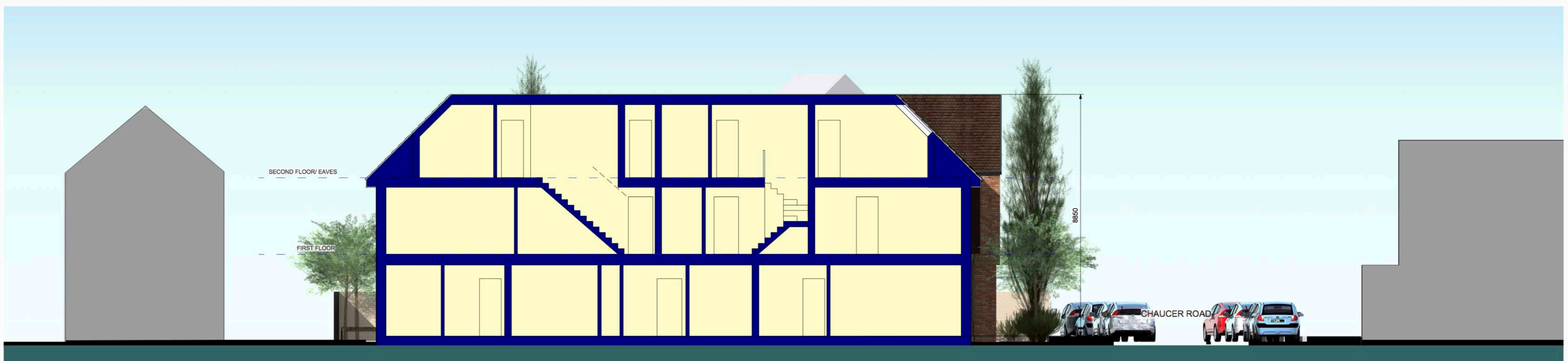




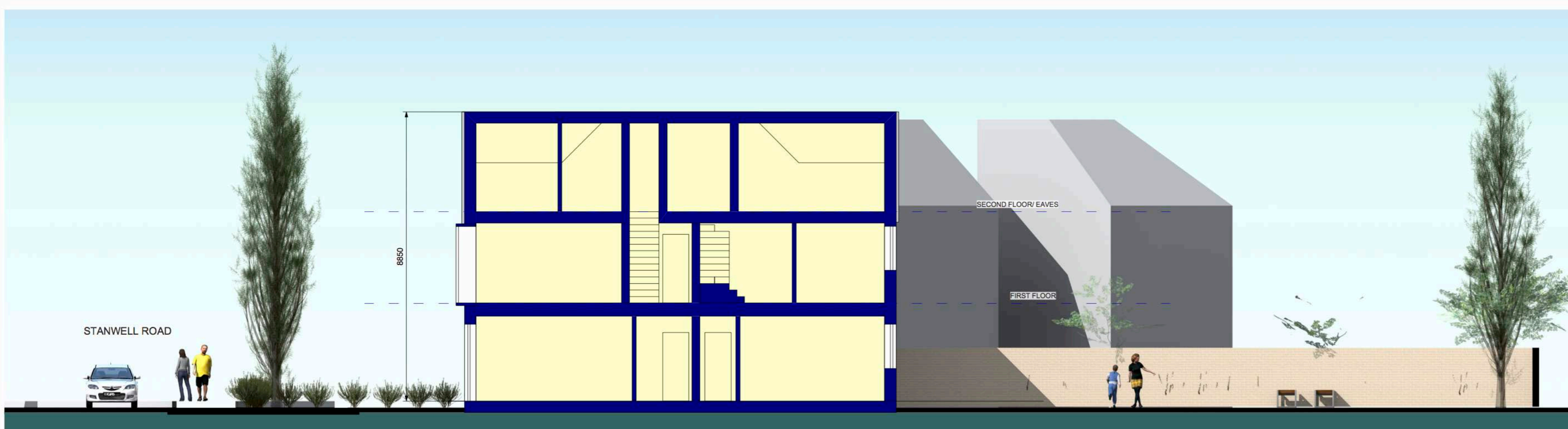
Reference plan



PROPOSED SECTION A-A



PROPOSED SECTION B-B



PROPOSED SECTION C-C



PROPOSED SECTION D-D

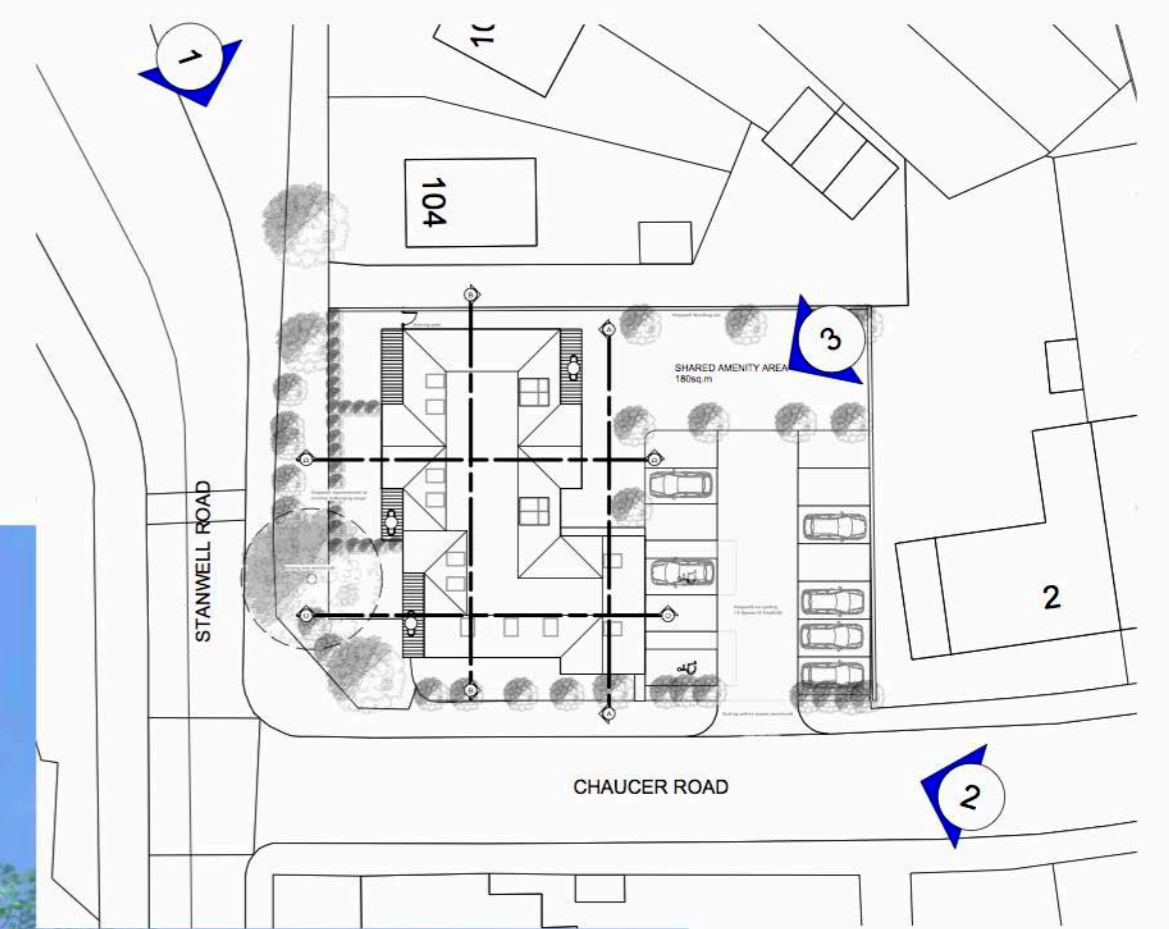
**PLANNING**

REV.	DATE	DESCRIPTION
PROJECT PROPOSED REDEVELOPMENT HEADLINE HOUSE STANWELL ROAD ASHFORD, TW15 3QH		
DRAWING PROPOSED SECTIONS AA, BB, CC, DD		
CLIENT LUMIERE PROPERTIES		
SCALE 1:100 @ A1	DRAWN	
DATE OCTOBER 2015	CHECK	
DRAWING No. 1537- PL1 /05	REVISION A	
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**LDA**  
Chartered Architects

LDA (Architects) Limited  
Lion House  
Oriental Road Woking  
Surrey. GU22 8AP  
T. 01932 828933  
E. lda@ldaarchitects.co.uk





Reference plan



1. 3D VIEW FROM STANWELL ROAD



2. 3D VIEW FROM CHAUCER ROAD



3. 3D VIEW FROM THE AMENITY AREA

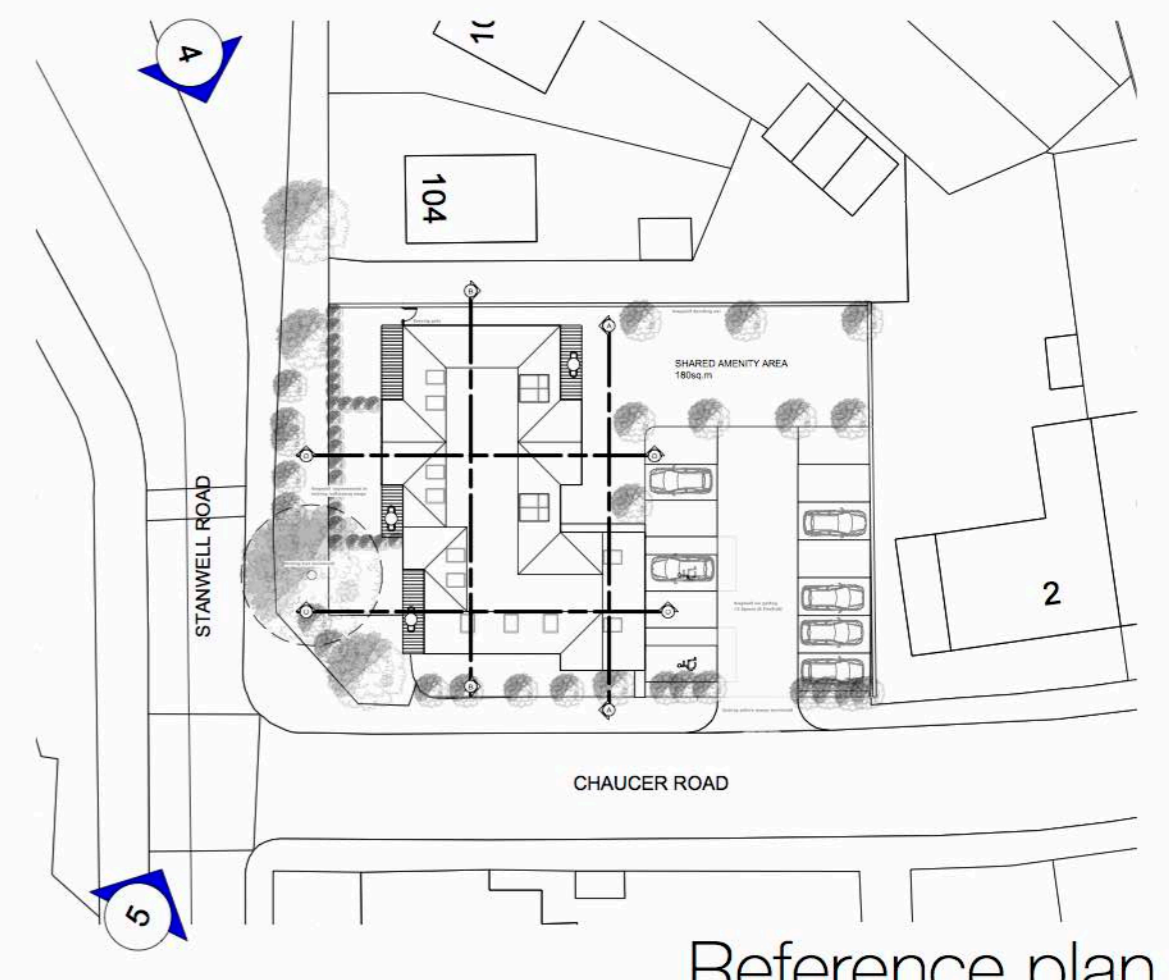
**PLANNING**

REV.	DATE	DESCRIPTION
PROJECT		
PROPOSED REDEVELOPMENT		
HEADLINE HOUSE		
STANWELL ROAD		
ASHFORD, TW15 3QH		
DRAWING		
3D VISUALS 1, 2 & 3.		
CLIENT		
LUMIERE PROPERTIES		
SCALE	NA	DRAWN
DATE	OCTOBER 2015	CHECK
DRAWING No.	1537- PL1 /06	REVISION
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**LDA**  
Chartered Architects

LDA (Architects) Limited  
Lion House  
Oriental Road  
Woking  
Surrey. GU22 8AP  
T. 01932 828933  
E. lda@ldaarchitects.co.uk





Reference plan



4. AERIAL VIEW FROM NORTH




5. AERIAL VIEW FROM SOUTH

## PLANNING

REV.	DATE	DESCRIPTION
PROJECT		
PROPOSED REDEVELOPMENT HEADLINE HOUSE STANWELL ROAD ASHFORD, TW15 3QH		
DRAWING		
AERIAL VIEWS 4 & 5		
CLIENT		
LUMIERE PROPERTIES		
SCALE	NA	DRAWN
DATE	OCTOBER 2015	CHECK
DRAWING No.	1537- PL1 /07	REVISION
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**LDA**  
Chartered Architects



LDA (Architects) Limited  
Lion House  
Oriental Road  
Woking  
Surrey, GU22 8AP

T. 01932 828933  
E. lda@ldaarchitects.co.uk



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BIRCH GREEN



15/01352/FUL

Venture House, 42-54 London Road,  
Staines-upon-Thames, TW18 4HF



1:1,250

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# Planning Committee

13 January 2016



<b>Application Nos.</b>	15/01352/FUL		
<b>Site Address</b>	Venture House, 42 – 54 London Road, Staines upon Thames		
<b>Proposal</b>	Removal of surface level car park and erection of a two-storey block of 6 flats (4 no. 1-bed and 2 no. 2-bed) together with associated amenity space		
<b>Applicant</b>	Mr Kunal Rawal		
<b>Ward</b>	Staines		
<b>Call in details</b>			
<b>Case Officer</b>	Paul Tomson		
<b>Application Dates</b>	Valid: 30.10.2015	Expiry: 25.12.2015	Target: Over 8 weeks
<b>Executive Summary</b>	<p>This application is seeks the removal of the existing surface level car park at the rear of the site and the erection of a 2-storey building to provide 6 flats. Whilst the site is located within a designated Employment Area, Prior Approval was granted in January 2015 to convert the offices to residential and it is therefore considered that the Council could not object to the loss of the commercial car parking area. The design and scale of the new building is in keeping with the character of the area. The development will have an acceptable relationship with the neighbouring residential properties in New Street and Sidney Road. Satisfactory levels of amenity space and parking will be provided. Furthermore, the scheme is considered acceptable in flood risk terms.</p>		
<b>Recommended Decision</b>	This application is recommended for approval		



## MAIN REPORT

### 1. Development Plan

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

- LO1 (Flooding)
- EM1 (Employment Development)
- HO4 (Housing Size and Type)
- HO5 (Housing Density)
- EN1 (Design of New Development)
- CC1 (Renewable Energy, Energy Conservation and Sustainable Construction)
- CC3 (Parking Provision)

### 2. Relevant Planning History

SP/97/0594	Redevelopment to provide 3-storey office building, with car parking (including basement car parking) and associated works.	Approved 22/04/1998
------------	--	------------------------

14/02058/PDO	Prior Approval Notification for the change of use from offices (Use Class B1a) to residential (Use Class C3) comprising 29 flats.	Prior Approval Given 20/01/2015
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*(Officer note: the approved plans show 28 units to be provided)*

15/00654/FUL	Removal of surface level car park and erection of a two-storey block of 6 flats (4 no. 1-bed and 2 no. 2-bed) together with associated amenity space.	Withdrawn 15/07/2015
--------------	---	-------------------------

*(Officer note: the above application was withdrawn because the Council raised concern regarding the design and scale of the building. The current proposal has been amended to address those concerns)*

### 3. Description of Current Proposal

3.1 The application relates to Venture House, 42 – 54 London Road, which is a 3-storey office building located on the southern side of the road. It has a basement car park, and a surface level car park located towards the rear of the site. The site is located within a designated Employment Area. It is also within an area liable to flood: Zone 2, which has between 1 in 100 year and 1 in 1000 year chance of flooding.

- 3.2 To the east is the adjacent 3-storey office block of Elizabeth House and its associated car park. To the west are the residential properties of 11 – 19 New Street. Further to the west on the other side of New Street is the commercial building of Prism House. To the south are the dwellings of 27 and 29 Sidney Road and their rear gardens.
- 3.3 The proposal involves the removal of the surface level car park and the erection of a 2-storey block of 6 flats. The proposed building will measure 16.3m in depth, 16m in width and up to 8.33m in height. It will be faced in buff coloured brickwork and grey coloured roof tiles. Private and communal amenity space will be provided. The proposed dwelling mix will comprise 4 no. 1-bed and 2 no. 2-bed units. 6 no. designated parking spaces (1 per unit) will be provided: 4 of them will be situated within the existing basement car park, whilst the other 2 will be located adjacent to the basement ramp. The parking provision for the remaining office building will be reduced from 59 to 24.
- 3.4 The scheme has been amended since it was first submitted. In particular, changes have been made to the roof with the introduction of a half-hip to the front elevation. In addition, the proposed site layout plan has been amended with the creation of a turning head to ensure that refuse vehicles can turn around within the site. As a consequence, the number of existing office parking spaces have been reduced from 27 to 24.
- 3.5 It is relevant to note that in January 2015 Prior Approval was granted under the Government's recent relaxation of Permitted Development rights to convert Venture House from Offices (Use Class B1a) to Residential (Use Class C3) to provide 28 flats. At the time of writing, the Prior Approval had not been implemented.
- 3.6 Copies of the proposed site layout, floor plans and elevations are provided as an Appendix.

#### 4. **Consultations**

- 4.1 The following table shows those bodies consulted and their response.

<b>Consultee</b>	<b>Comment</b>
County Highway Authority	No objection
Environmental Health (Pollution)	No objection subject to conditions relating to contaminated land. No objection on air quality grounds, but recommends that the cycle parking provision on the overall site is increased
Environment Agency	No objection subject to conditions
Street Scene	No objection to the revised site layout plan showing the provision of a turning head. Has made detailed comments regarding extra signage, removal of existing bollards, dropped kerbs for disabled

	access and other information, which has been forwarded to the applicant.
County Archaeologist	No objection subject to a condition

## 5. **Public Consultation**

5.1 31 neighbouring properties were notified of the planning application. 2 letters of objection has been received (from 29 and 31 Sidney Road). Issues raised include:

- Loss of privacy/overlooking. Mature trees should be planted in the communal garden to screen views from the flats' windows.
- Loss of light
- Concern about flooding.

## 6 **Planning Issues**

- Loss of designated Employment Land/Principle
- Housing size and type
- Housing density
- Design and appearance
- Impact on neighbouring residential properties
- Amenity space
- Parking/Loss of existing parking
- Flooding

## 7 **Planning Considerations**

### Loss of designated Employment Land/Principle

7.1 The site is located within a designated Employment Area. Policy EM1 of the Core Strategy and Policies DPD 2009 (CS & P DPD) stipulates that there shall be no loss of employment land or floorspace within an Employment Area. Whilst the proposal will result in the loss of part of the site (i.e. the surface level car park) to residential, it is not considered that the Council could resist the loss of this particular area of designated employment. As mentioned above, in January 2015 Prior Approval was granted for the change of use of the entire site from offices to residential comprising 28 flats. Both the basement and surface level car park was to be used for parking in association with the new flats. Whilst it is likely the applicant would have to re-apply for a revised Prior Approval application with a reduction in the level of car parking (from 59 to 24), the loss of the employment use of the site and the principle of residential development has already been accepted.

### Housing Density

7.2 Policy HO5 of the CS & P DPD states that within higher density residential areas, including those characterised by a significant proportion of flats and those containing significant Employment Areas, new development should generally be in the range of 40 – 75 dwellings per hectare (dph). Higher

density development may be acceptable where it is demonstrated that the development complies with Policy EN1 of the CS & P DPD on design, particularly in terms of its compatibility with the character of the area and is in a location that is accessible by non car-based modes of travel.

- 7.3 Whilst the main part of the site is still occupied as offices, the proposed residential density for the southern part of the site where the block of flats is to be sited is 105 dph. However, this does not include the area covered by the proposed designated parking spaces or the access thereto, which would if included, lower the density level. If the existing office building were to be converted to residential and the proposed block of 6 flats implemented, the residential density for the overall site would 170 dph. The proposed building and associated amenity space is considered to be acceptable in design terms and complies with Policy EN1, which is explained in detail in the following paragraphs. Moreover, the site is in a location that is accessible by non car-based modes of travel. A consequence of higher residential density is that a lower parking provision is provided compared to that stipulated in the Council's Parking Standards. However, limited parking provision may still be acceptable in town centre locations with access to non car-based modes of transport. Consequently, the proposed housing density is considered acceptable in this particular location.

#### Design and Appearance

- 7.4 Policy EN1a of the CS & P DPD states that the Council will require a high standard in the design and layout of new development. Proposals for new development should demonstrate that they will create buildings and places that are attractive with their own distinct identity; they should respect and make a positive contribution to the street scene and the character of the area in which they are situated, paying due regard to the scale, height, proportions, building lines, layout, materials and other characteristics of adjoining buildings and land.
- 7.5 It is considered that the proposed design and appearance is acceptable and is in keeping with the character with the area. The new building will be set back from the London Road frontage and there will be only limited views of it through the gap between the existing office buildings. The proposed building will be faced with buff brickwork and grey coloured tiles to match Venture House. Furthermore, it will have a pitched roof design and be 2-storey in scale, and therefore appear in keeping with the 2-storey residential buildings to the west in New Street. The proposed western elevation is set in from the side boundary by only 0.2 metres, which is less than the normal minimum 1 metre set-in standard stipulated in the Supplementary Planning Document on Residential Extensions and New Residential Development 2011 (SPD). However, the site is not within a traditional residential street scene and there will be no issue of a potential terracing effect. Consequently, the limited set-in from the boundary is considered acceptable and will not otherwise harm the character of the area.

### Impact on Neighbouring Residential Properties

- 7.6 Policy EN1b of the CS & P DPD states that new development should achieve a satisfactory relationship to adjoining properties avoiding significant harmful impact in terms of loss of privacy, daylight or sunlight, or overbearing effect due to bulk and proximity or outlook.
- 7.7 The proposal is considered to have an acceptable relationship with neighbouring residential properties. There will be a satisfactory separation distance between the proposed 2-storey western flank elevation and the rear elevations of 11 – 17 New Street. For example, there will be a gap of at least 15.2 metres between the original 2-storey rear elevation of 15 New Street, whilst the distance will be 17.9 metres between the rear elevation of the maisonettes of 11 & 13 New Street. These distances are above the minimum 13.5m “back to flank” separation distance standard stipulated in the SPD. With regard to 27 and 29 Sidney Road, there will be gap of at least 10.7m between the proposed rear elevation and the southern boundary. This is above the minimum 10.5m “back to boundary” standard in the SPD. Consequently, it is considered that there will not be any undue loss of amenity. I also consider the relationship with the office building of Elizabeth House to be acceptable.

### Amenity Space

- 7.8 The proposed development comprises a communal garden at the southern end of the site measuring 162 sqm. In addition, 2 small private amenity areas/terraces are to be provided for the ground floor rear flats, each measuring 16 sqm. The combined size of the communal and private amenity spaces/terraces is 194 sqm, which is above the Council minimum amenity space standard of 185 sqm for scheme of this size. Moreover, the garden areas face a southerly direction and therefore have a high amenity value. Accordingly, the proposed amenity space is considered acceptable.

### Parking Provision

- 7.9 Policy CC3 (Parking Provision) of the CS & P DPD states that the Council will require appropriate provision to be made for off-street parking in development proposals in accordance with its maximum parking standards.
- 7.10 On 20 September 2011 the Council's Cabinet agreed a 'Position Statement' on how Policy CC3 should now be interpreted in the light of the Government's recent parking policy changes. The effect of this is that the Council will give little weight to the word 'maximum' in relation to residential development when applying Policy CC3 and its residential parking standards will generally be applied as minimum (maximum parking standards continue to be applicable in relation to commercial development). The supporting text to the Parking Standards stipulates a number of exceptional situations where a reduction in parking will only be allowed. One of these situations includes town centre locations where the reduction parking will be assessed against the distance from a "public transport node", frequency of public transport, availability of pedestrian and cycle routes, and the range and quality of facilities supportive of residential development within reasonable walking distance.

- 7.11 The proposed parking provision is 6 spaces (1 space per unit). As mentioned above, these designated spaces are to be located within the existing basement and adjacent to the basement ramp. The normal minimum parking standard for a scheme of this size is 8. The proposed provision is therefore 2 spaces below the standard. The site is located however, within the commercial area of Staines town centre. It is close to the train station which has a frequent and extensive service. It is also within easy walking distance of the shops and services of the town centre (which has the largest range of facilities in the Borough) and the bus station. There are also several bus services in London Road. Consequently, I consider that there is clear justification for a reduction in the level of parking on this particular site.
- 7.12 It is not considered that an objection could be raised to the reduced parking level provided for the remaining office building of Venture House in this town centre location. The remaining number of office parking spaces would be 24. The Council's Maximum Parking Standards (note maximum standards are applied for commercial development, not minimum) stipulate that within Staines town centre employment area provision should not normally exceed 1 car space per 50 sqm gross floorspace. The existing building has 1890 sqm of office floorspace (as stated by the applicant in the previous Prior Approval application) and the proposed commercial car parking provision would be 1 space per 79 sqm. As this is below the maximum standard of 1 space per 50 sqm, the proposed commercial parking level is acceptable.
- 7.13 As mentioned above, if the applicant were to subsequently decide to implement the conversion of the existing office building to residential (and build the proposed development), it is likely they would have to re-apply for a revised Prior Approval application due to the reduced level of parking provision from 59 to 24. This revised parking provision would equate to just under 1 space per unit (0.86). The Council's parking standards would normally stipulate a minimum provision of 39 spaces (a shortfall of 15 spaces). However, as previously stated, the site is in a town centre location where a more flexible approach to parking provision is acceptable. It is not considered that an objection could be raised on parking/highway grounds if this particular scenario were to be carried out on the site. Likewise, the remaining provision of 24 parking spaces for the remaining office building would be acceptable in this location. There is extensive single and double line parking restrictions in the vicinity of the site, which would control and make safe any on-street parking demand.

#### Flooding

- 7.14 The site is located within Flood Zone 2, which has between 1 in 100 year and 1 in 1000 year chance of flooding. There is no objection in principle to the creation of new residential development in this relatively low risk flood zone. The Environment Agency were consulted and have raised no objection subject to conditions relating to contaminated land and groundwater. In addition conditions are to be imposed to prevent any raising of existing ground levels and requiring the removal of any spoil from construction. Accordingly, the impact on flood risk is considered acceptable.

## Other Matters

- 7.15 The development complies with Policy HO4 of the CS & P DPD which requires developments to include at least 80% of their total as one or two bedroom units.
- 7.16 With regard to the representation from the third party, it is not considered that the Council could insist on the planting of mature trees along the rear boundary, particularly as the new building complies with the separation distances standards in the SPD. However, conditions will be imposed requiring details of boundary treatment and landscaping to be submitted for approval, which should ensure that the rear boundary is treated in a satisfactory manner.
- 7.17 The Council's Street Scene has confirmed the size of the bin store is adequate. Furthermore, they are satisfied with the revised site layout plan showing the provision of a turning head. A condition is recommended to ensure that the turning head is implemented and maintained on the site.
- 7.18 The floorspace of the proposed units comply with the minimum standards stipulated in the SPD.
- 7.19 In terms of cycling parking facilities, space for 6 bicycles is provided in a covered area in the new block. This represents 1 cycle parking space per unit which is considered acceptable and accords with the requirement in the Council's Parking Standards.
- 7.20 Accordingly, the application is recommended for approval.

## **8. Recommendation**

8.1 GRANT subject to the following conditions:-

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:- This condition is required by Section 91 of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans and drawings:

A00-(00) received 12 October 2015.

A00-07 received 29 October 2015.

A00-02 Rev. D; /03 Rev. D; /04 Rev. E; /05 Rev. E; /06 Rev. E received 01 December 2015

A00-01 Rev. C received 07 December 2015

Reason:- For the avoidance of doubt and in the interest of proper planning

3. Before any work on the development hereby permitted is first commenced details of the materials and detailing to be used for the external surfaces of the buildings be submitted to and approved by the Local Planning Authority.

Reason:- To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

4. No development shall take place until:-
  - (a) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site, has been submitted to and approved in writing by the Local Planning Authority.
  - (b) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.
  - (c) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

Reason:-

- (a) To protect the amenities of future residents and the environment from the effects of potentially harmful substances.

#### NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination: Guidance to Help Developers Meet Planning Requirements" proving guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).



In accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

5. Prior to the first use or occupation of the development, and on completion of the agreed contamination remediation works, a validation report that demonstrates the effectiveness of the remediation carried out shall be submitted to and agreed in writing by the Local Planning Authority.

Reason:- To protect the amenities of future residents and the environment from the effects of potentially harmful substances.

6. No development shall commence until a report has been submitted to and agreed by the Local Planning Authority which includes details and drawings demonstrating how 10% of the energy requirements generated by the development as a whole will be achieved utilising renewable energy methods and showing in detail the estimated sizing of each of the contributing technologies to the overall percentage. The detailed report shall identify how renewable energy, passive energy and efficiency measures will be generated and utilised for each of the proposed buildings to meet collectively the requirement for the scheme. The agreed measures shall be implemented with the construction of each building and thereafter retained and maintained to the satisfaction of the Local Planning Authority unless otherwise agreed in writing.

Reason:- To ensure that the development is sustainable and complies with Policy SP7 and CC1 of the Spelthorne Development Plan Core Strategy and Policies DPD.

7. No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a plan indicating the positions, design, materials and type of boundary treatment to be erected. The details shall include plans and elevations of the proposed sliding entrance gates. The boundary treatment and gates shall be completed before the buildings are occupied. Development shall be carried out in accordance with the approved details and maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

8. No development shall take place until full details of both soft and hard landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. The trees, shrubs and other associated proposals shall be planted on the site within a period of 12 months from the date on which development hereby permitted is first commenced, or such longer period as may be approved by the Local Planning Authority, and that the planting so provided shall be maintained as approved for a period

of 5 years, such maintenance to include the replacement in the current or next planting season whichever is the sooner, of any trees or shrubs that may die, are removed or become seriously damaged or diseased, with others of similar size and species, unless the Local Planning Authority gives written permission to any variation.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

9. Before the first occupation of any part of the development, a landscape management plan including long-term design objectives, management responsibilities and maintenance schedules for all landscaped areas shall be submitted to and approved in writing by the Local Planning Authority. The landscape management plan shall be carried out as approved.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

10. The 6 no. allocated car parking spaces and access/turning area thereto, as shown on approved drawing no. A00-07 received 29/10/2015, shall be permanently maintained for their designated purpose.

Reason:- The above condition is required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users in accordance with policy CC2 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

11. No development shall take place until a Method of Construction Statement, to include details of:
  - (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) measures to keep the highway clear from deposit of site materials
  - (e) measures to minimise dust during constructionhas been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction period.

Reason:- In the interest of highway safety and to minimise dust pollution during construction.

12. No new development shall be occupied until space has been laid out in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority to provide secure and covered cycle parking for both the residential development hereby permitted and the remaining development at Venture House, and thereafter shall be maintained and approved.

Reason:- The above condition is required in recognition of Section 4 (Promoting Sustainable Transport) of the NPPF

13. Before any development commences, details including a technical specification of all proposed external lighting shall be submitted to and approved in writing by the Local Planning Authority. The external lighting on the site shall at all times accord with the approved details.

Reason:- To safeguard the amenity of neighbouring residential properties and in the interest of security.

14. That within 3 months of the commencement of any part of the development permitted, or such longer period as may be approved by the Local Planning Authority, facilities shall be provided within the curtilage of the site for the storage of refuse and waste materials in accordance with the details hereby approved, and thereafter shall be maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

15. No development shall take place until the applicant has secured the implementation of a programme of archaeological work in accordance with a Written Scheme of Investigation which has been submitted by the applicant and approved by the Local Planning Authority.

Reason:- The site is of high archaeological potential and it is important that the archaeological information should be preserved as a record before it is destroyed by the development.

16. No infiltration of surface water drainage into the ground within the development site is permitted other than with the express written consent of the local planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to controlled waters. The development shall be carried out in accordance with the approval details.

Reason:- To prevent pollution of the water environment due to possible migration of contaminants from within the contaminated land. National Planning Policy Framework paragraph 109 states that the planning system should contribute to and enhance the natural and local environment by preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels water pollution. This condition is also sought in accordance with part (d) of policy EN9 of Spelthorne Borough Council's local plan adopted in 2009.

17. Piling or any other foundation designs using penetrative methods shall not be permitted other than with the express written consent of the local

planning authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason:- Piling or any other foundation designs using penetrative methods can result in risks to potable supplies from, for example, pollution / turbidity, risk of mobilising contamination, drilling through different aquifers and creating preferential pathways. Thus it should be demonstrated that any proposed piling will not result in contamination of groundwater. The National Planning Policy Framework paragraph 109 states that the planning system should contribute to and enhance the natural and local environment by preventing both new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by unacceptable levels water pollution.

18. There shall be no raising of existing ground levels on the site within the area liable to flood, other than in accordance with the approved details.

Reason: -To prevent the increased risk of flooding due to impedance of flood flows and reduction in flood storage capacity in accordance with policies SP1, SP7 and LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

19. All spoil and building materials stored on site before and during construction shall be removed from the area of land liable to flood upon completion

Reason:- To prevent the increased risk of flooding due to impedance of flood flows and reduction of flood storage capacity in accordance with policies SP1, SP7 and LO1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

20. Prior to the commencement of development, details of the works to be carried out on the site to create the turning head and revised disabled parking space shall be submitted to and approved in writing by the Local Planning Authority. The details shall include amendments to the footway and measures to prevent cars parking in the approved turning head. The approved works and measures shall be carried out in accordance with the agreed details before the occupation of the new building, and thereafter maintained.

Reason: To ensure that a refuse vehicle and other service vehicles can enter and exit the site in forward gear, in the interest of highway safety.

### Informatives

1. Please note that this application is subject to the payment of Community Infrastructure Levy (CIL). Full details of the charge, how it has been calculated and what happens next are set out in the CIL Liability Notice which will be sent separately.

If you have not already done so an Assumption of Liability notice should be sent to the Council as soon as possible and before the commencement of development.

Further information on CIL and the stages which need to be followed is available on the Council's website. [www.spelthorne.go.uk/CIL](http://www.spelthorne.go.uk/CIL).

2. The applicant is advised that the site to which this planning permission relates is located on or near land that may contain harmful substances. Under Part C of the Building Regulations you will be required to consider this when designing the foundations of the development.

The applicant is advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences.

### **Article 2(3) Development Management Procedure (Amendment) Order 2012**





In assessing this application, officers have worked with the applicant in a positive and proactive manner consistent with the requirements of paragraphs 186-187 of the NPPF. This included the following:-

- a) Provided pre-application advice to seek to resolve problems before the application was submitted and to foster the delivery of sustainable development.
- b) Provided feedback through the validation process including information on the website, to correct identified problems to ensure that the application was correct and could be registered;
- c) Have suggested/accepted/negotiated amendments to the scheme to resolve identified problems with the proposal and to seek to foster sustainable development.
- d) Have proactively communicated with the applicant through the process to advise progress, timescales or recommendation.



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KEY

- Timber Fence 
- Site Boundary 
- Lighting Path 
- Security Gate 

NOTES

The boundary to the front of the site will be treated with a 450mm high timber knee rail fence. The boundary to the sides and rear of the site will be treated via the use of a timber closeboarded fence, min 2.0m high in compliance with Secure by Design.

The front of the site will be finished with either hardscape or grass and shrub planting.

Controlled access to the rear of the property will be achieved by the installation of a gate at the side with access control to the apartments accessed from the rear.

Flats facing onto the rear will be provided with a private area. This area to be defined by use of a low level timber fence. The rear of the site will have an area of communal garden for enjoyment by each of the flats. This area will be primarily finished with grass and shrub planting.

Path of lighting is indicative, design TBC, spec to be eco friendly and low wattage.

All doors and windows to be PAS 24 rated.

Nb.  
 Site plan currently based on OS data. A further topographical survey required to determine site boundary.

No	Date	Description	By	Ckd
C	07.12.15	Parking amended to accommodate refuse vehicle path.	AM	DRJ
B	02.10.15	Drawing number amended.	AM	DRJ
A	09.09.15	Site note expanded	SH	RH

CLIENT



PROJECT  
 Proposed Resi to rear of 42  
 London Rd, Staines Upon  
 Thames  
 TITLE  
 Proposed Site Plan

DRAWING STATUS  
 For Information

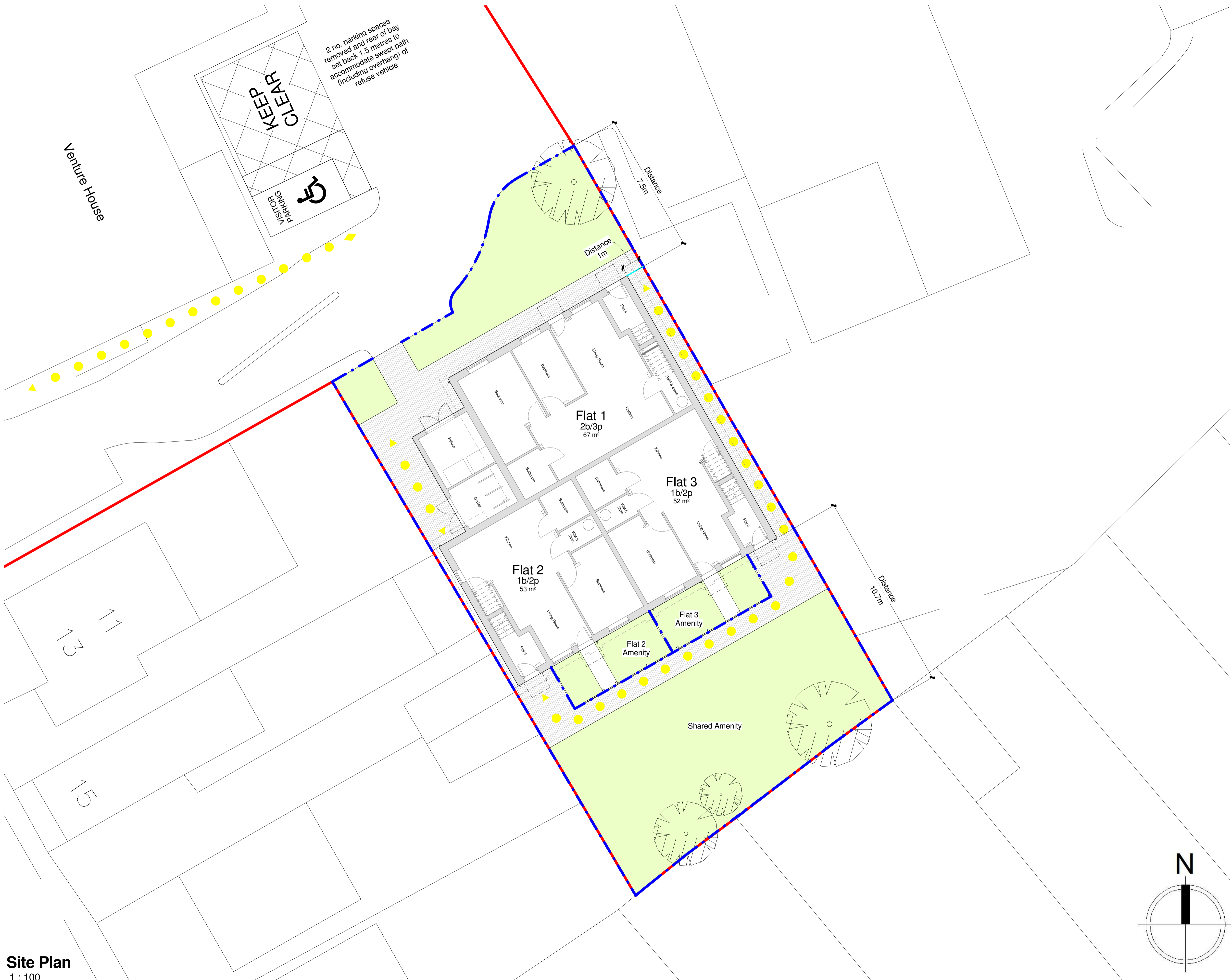
DRAWN	SH	CHECKED	RH

SCALE As indicated @ A1  
 DATE 09/08/15

**wcec** architects  
 South Building, Staple Inn Buildings  
 London, WC1V 7PZ

t:020 3388 0019, e: email@wcec.co.uk, www.wcec.co.uk

JOB NO.	DRAWING NO.	REV
13-521	A00-01	C



2 no. parking spaces removed and rear of bay set back 1.5 metres to accommodate swept path (including overhang) of refuse vehicle

Venture House

KEEP CLEAR  
 VISITOR PARKING

Flat 1  
 2b/3p  
 67 m<sup>2</sup>

Flat 3  
 1b/2p  
 52 m<sup>2</sup>

Flat 2  
 1b/2p  
 53 m<sup>2</sup>

Flat 3  
 Amenity

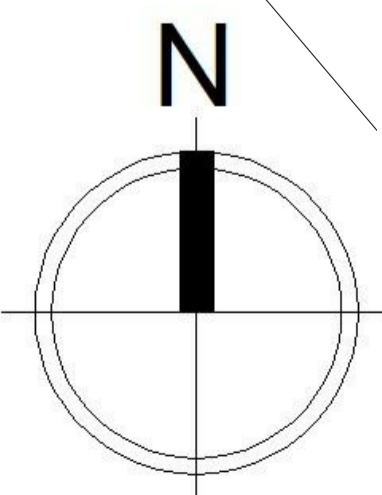
Flat 2  
 Amenity

Shared Amenity

Distance  
 7.5m

Distance  
 1m

Distance  
 10.7m



Site Plan  
 1 : 100



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**Life Time Homes Legend**

**Criterion 4 – Entrances**

- All entrances should:  
 a) Be illuminated  
 b) Have level access over the threshold; and  
 c) Have effective clear opening widths and nibs as specified below.  
 In addition, main entrances should also:  
 d) Have adequate weather protection\*  
 e) Have a level external landing.\*

**Criterion 6 – Internal doorways and hallways**

Movement in hallways and through doorways should be as convenient to the widest range of people, including those using mobility aids or wheelchairs, and those moving furniture or other objects.

As a general principle, narrower hallways and landings will need wider doorways in their side walls.

The width of doorways and hallways should conform to the specification below.

**Criterion 7 – Circulation Space**

There should be space for turning a wheelchair in dining areas and living rooms and basic circulation space for wheelchair users elsewhere.

**Criterion 8 – Entrance level living space**

A living room / living space should be provided on the entrance level of every dwelling

**Criterion 13 – Potential for fitting of hoists and bedroom / bathroom relationship**

Structure above a main bedroom and bathroom ceilings should be capable of supporting ceiling hoists and the design should provide a reasonable route between this bedroom and the bathroom.

**Criterion 14 – Bathrooms**

An accessible bathroom, providing ease of access in accordance with the specification below, should be provided in every dwelling on the same storey as a main bedroom.

**Criterion 15 – Glazing and window handle heights**

Windows in the principal living space (typically the living room), should allow people to see out when seated. In addition, at least one opening light in each habitable room should be approachable and usable by a wide range of people – including those with restricted movement and reach

No	Date	Description	By	Ckd
D	01.12.15	Roof updated to introduce hip to North Elevation. PV moved to South Elevation.	AM	DRJ
C	09.09.15	Flat 4 cupboard door repositioned	SH	RH
B	08.09.15	Drawing title amended	SH	RH
A	02.09.15	Alterations to internal layouts with the inclusion of Lifetime Homes	SH	RH

CLIENT



SETTING TRENDS IN PROPERTY DEVELOPMENT

PROJECT  
 Proposed Resi to rear of 42  
 London Rd, Staines Upon  
 Thames  
 TITLE  
 Proposed GA Floor Plans

DRAWING STATUS  
 For Information

DRAWN AM CHECKED DRJ

SCALE As Indicated @ A1

DATE 03/12/15



South Building, Staple Inn Buildings  
 London, WC1V 7PZ

t:020 3388 0019, e: email@wcec.co.uk, www.wcec.co.uk

JOB NO.	DRAWING NO.	REV
13-521	A00-02	D

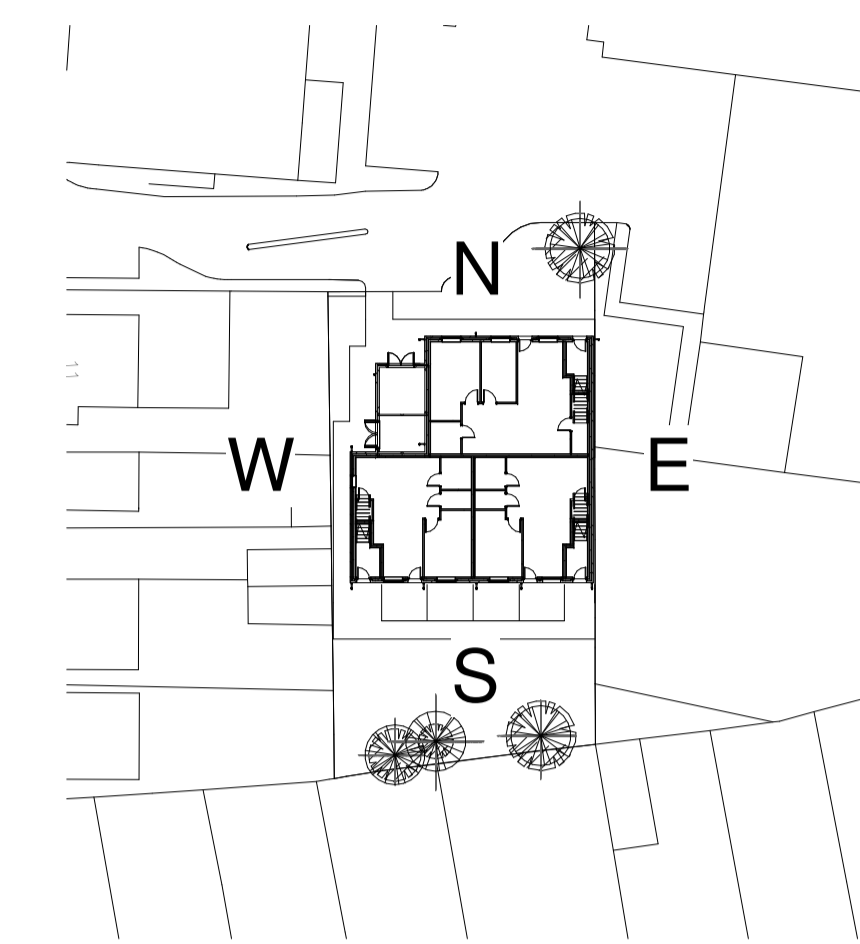


Room Schedule			
Number	Occupancy	Area	Area (Sq Ft)
Flat 1	2b/3p	67 m <sup>2</sup>	722 SF
Flat 2	1b/2p	53 m <sup>2</sup>	567 SF
Flat 3	1b/2p	52 m <sup>2</sup>	557 SF
Flat 4	2b/3p	71 m <sup>2</sup>	762 SF
Flat 5	1b/2p	53 m <sup>2</sup>	566 SF
Flat 6	1b/2p	51 m <sup>2</sup>	553 SF



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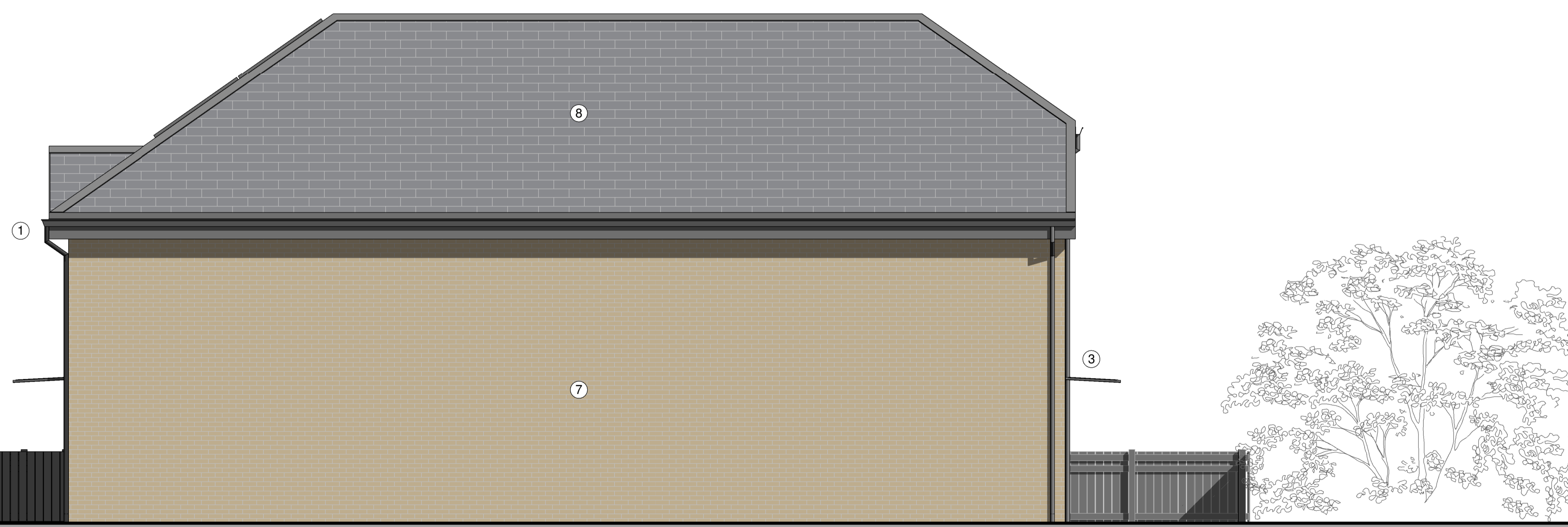
Roof Ridge 8330  
 Roof 4650  
 1st FL 3150  
 GFL 150



- KEY**
- 1 - Aluminium Rain water Goods
  - 2 - Aluminium Coping Trim
  - 3 - Glass/ Polycarbonate Canopy
  - 4 - PV Array
  - 5 - Obscure Glass Window Fixed Light
  - 6- Timber Effect Cladding
  - 7- Buff Brickwork to Match Venture House
  - 8- Grey Fibre Cement Tile

**South Elevation**  
 1 : 50

Roof Ridge 8330  
 Roof 4650  
 1st FL 3150  
 GFL 150



No	Date	Description	By	Ckd
E	01.12.15	Roof updated to introduce hip to North Elevation. PV moved to South Elevation.	AM	DRJ
D	09.09.15	Timber fence and finishes added to elevations	SH	RH
C	08.09.15	Alterations to roof	SH	RH
B	02.09.15	Alterations to Fenestration	SH	RH
A	24.04.15	Levels raised 150mm in line with flood prevention.	AM	DRJ

CLIENT  
**ASHBURY & BLOOM**  
 SETTING TRENDS IN PROPERTY DEVELOPMENT

PROJECT  
 Proposed Resi to rear of 42 London Rd, Staines Upon Thames  
 TITLE  
 Proposed GA Elevations

DRAWING STATUS  
 For Information  
 DRAWN AM CHECKED DRJ  
 SCALE As indicated @ A1  
 DATE 03/12/15

**wcec** architects  
 South Building, Staple Inn Buildings  
 London, WC1V 7PZ  
 t:020 3388 0019, e: email@wcec.co.uk, www.wcec.co.uk

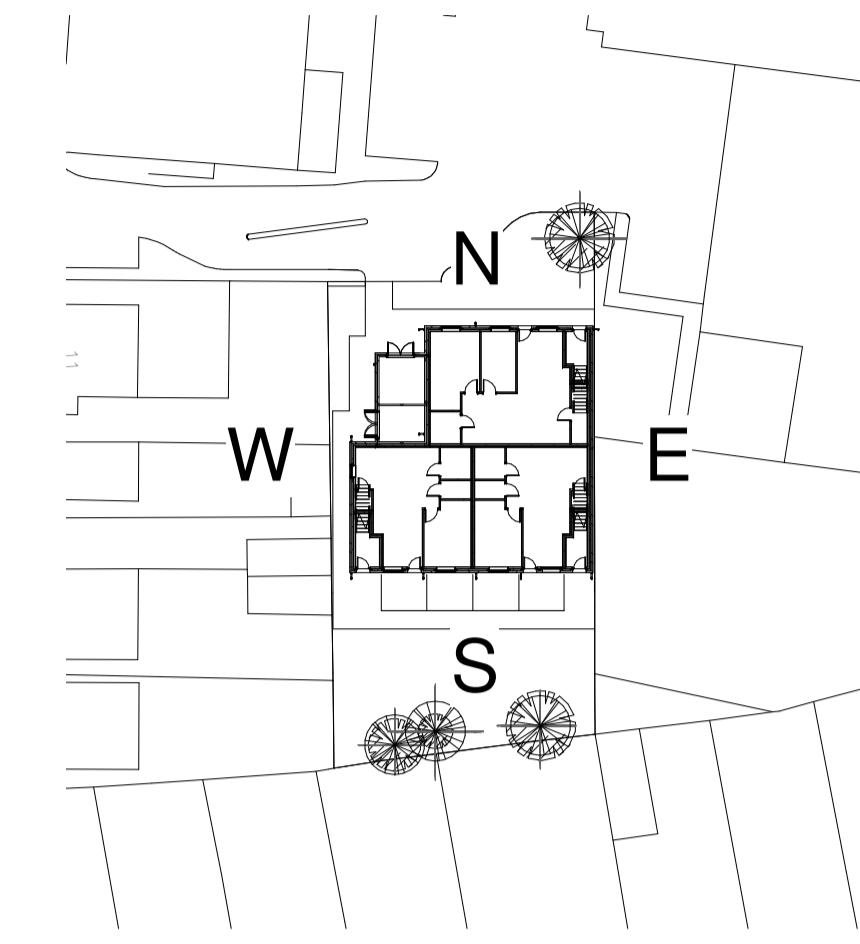
JOB NO.	DRAWING NO.	REV
13-521	A00-04	E

**East Elevation**  
 1 : 50



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Roof Ridge 8330  
 Roof 4650  
 1st FL 3150  
 GFL 150



- KEY**
- 1 - Aluminium Rain water Goods
  - 2 - Aluminium Coping Trim
  - 3 - Glass/ Polycarbonate Canopy
  - 4 - PV Array
  - 5 - Obscure Glass Window Fixed Light
  - 6 - Timber Effect Cladding
  - 7 - Buff Brickwork to Match Venture House
  - 8 - Grey Fibre Cement Tile

**North Elevation**  
 1 : 50

Roof Ridge 8330  
 Roof 4650  
 1st FL 3150  
 GFL 150



No	Date	Description	By	Ckd
E	01.12.15	Roof updated to introduce hip to North Elevation. PV moved to South Elevation.	AM	DRJ
D	09.09.15	Timber fence and finishes added to elevations	SH	RH
C	08.09.15	Alterations to roof	SH	RH
B	02.09.15	Alterations to Fenestration	SH	RH
A	24.04.15	Levels raised 150mm in line with flood prevention.	AM	DRJ

CLIENT  
**ASHBURY & BLOOM**  
 SETTING TRENDS IN PROPERTY DEVELOPMENT

PROJECT  
 Proposed Resi to rear of 42  
 London Rd, Staines Upon  
 Thames  
 TITLE  
 Proposed GA Elevations 2

DRAWING STATUS  
 For Information  
 DRAWN AM CHECKED DRJ  
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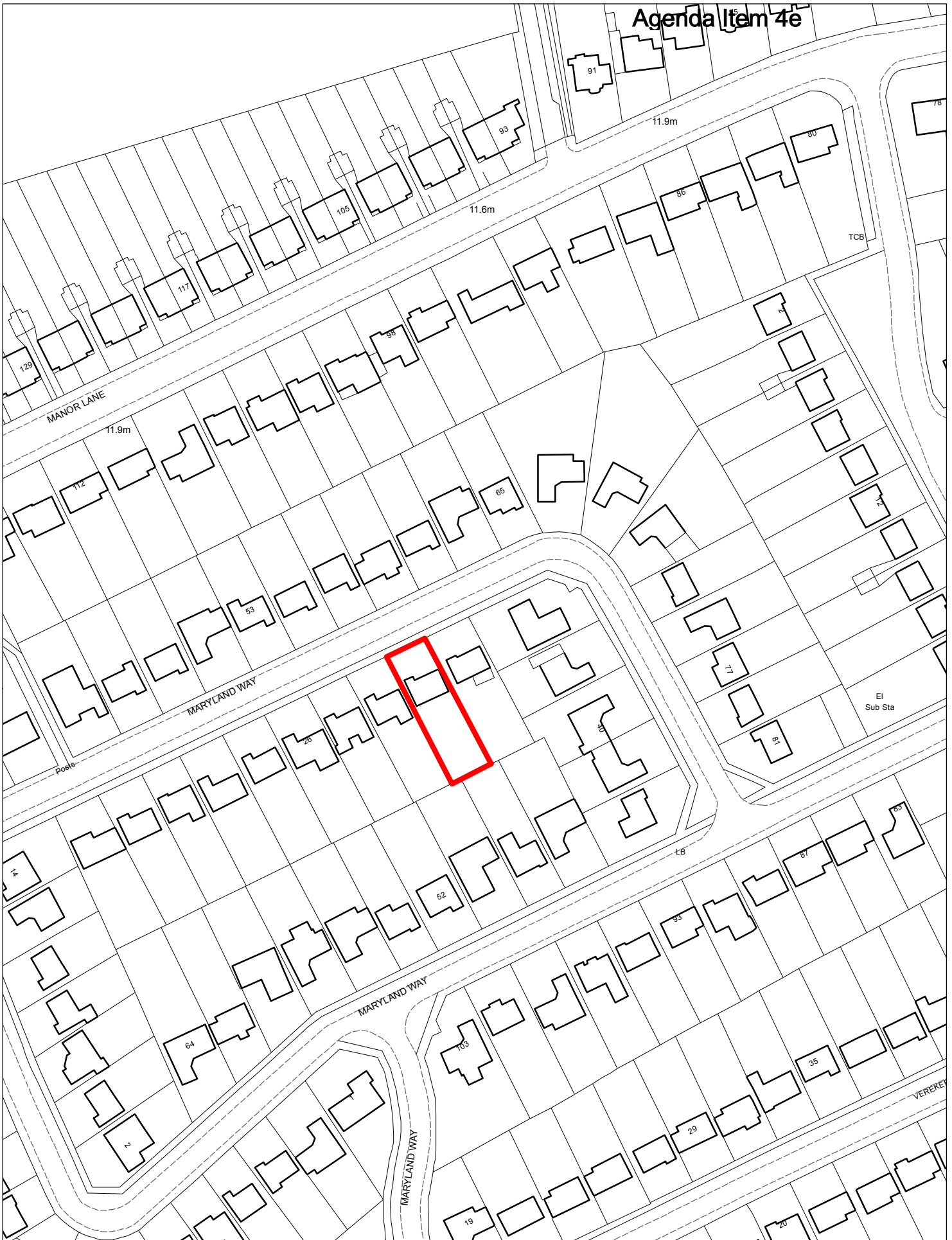
**wcec** architects  
 South Building, Staple Inn Buildings  
 London, WC1V 7PZ  
 t:020 3388 0019, e: email@wcec.co.uk, www.wcec.co.uk

JOB NO.	DRAWING NO.	REV
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**West Elevation**  
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**15/01343/FUL**  
**32 Maryland Way, Sunbury-on-Thames, TW16 6HR**

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## Planning Committee

13 January 2016



<b>Application Nos.</b>	15/01343/HOU		
<b>Site Address</b>	32 Maryland Way, Sunbury-on-Thames TW16 6HR		
<b>Proposal</b>	Erection of a two storey rear extension.		
<b>Applicant</b>	Mr P. Bonner		
<b>Ward</b>	Sunbury East		
<b>Call in details</b>	The application has been called in by Cllr. Friday due to concerns of local residents on the potential impacts of the proposal.		
<b>Case Officer</b>	Matthew Clapham		
<b>Application Dates</b>	Valid: 08.10.2015	Expiry: 03.12.2015	Target: Over 8 weeks
<b>Executive Summary</b>	<p>This application seeks the erection of a two storey rear extension. The extension would be across the entire existing width of the dwelling and extending to the rear by 3.75m replacing an existing single storey flat roofed extension. The proposal has been redesigned and reduced in size compared to a previously withdrawn scheme (15/00804/HOU). The roof has been designed with hipped sides to slope away from the side boundaries and the proposal has an acceptable impact on the amenity of adjoining residential properties. The overall design, height and scale of the development now respects the design and proportions of the original house. Consequently, the impact on the character and appearance of the area is considered acceptable.</p> <p>The proposal complies with Policy EN1 (Design of New Development) of the Spelthorne Borough Core Strategy and Policies DPD (2009) and the guidance contained in the Councils Supplementary Planning Document on the Design of Residential Extensions and New Residential Development (2011).</p>		
<b>Recommended Decision</b>	The application is recommended for approval.		

## MAIN REPORT

### 1. Development Plan

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

➤ EN1 (Design of New Development)

### 2. Relevant Planning History

15/00804/HOU	Erection of a two storey rear extension.	Withdrawn 29.07.2015
09/00158/HOU	Erection of part single storey, part two-storey rear extension following demolition of brick-built outbuilding.	Application Refused 21.05.2009
08/00806/HOU	Erection of single storey rear extension with pitched roof over. Installation of 2 side dormers to create loft conversion following demolition of existing single storey rear extension.	Application Refused 13.11.2008

### 3. Description of Current Proposal

- 3.1 This application site is located on the southern side of Maryland Way and is currently occupied by a detached two storey dwelling. The area is characterised by similarly designed detached two storey dwellings, the design and style of which is known as the 'Berg Estate'.
- 3.2 It is proposed to erect a two storey rear extension. The extension projects 3.75m to the rear and across the entire width of the existing dwelling. The roof height matches the existing ridge height in the form of a double pitched roof design to the rear. The eaves height to the rear is 5.3m. To the side the extension has been designed to have sloping hipped roofs away from the side boundaries. The eaves height to the sides is reduced to 3.4m. A rooflight is also proposed on the eastern side serving a new bathroom.
- 3.3 An application was made earlier this year (15/00804/HOU) by the same applicant for a two storey rear extension which was withdrawn. This proposed a larger fully two storey extension with eaves heights to the side and rear of 5.1m.
- 3.4 Previously in 2008 and 2009, two proposals from a different applicant were refused. The most recent (09/00158/HOU) being for the following reason:

*The design of the proposed extension has insufficient regard to the design of the host building and would appear unduly obtrusive when viewed from the adjoining properties and would result in a poor outlook from and have an overbearing effect upon no 34 Maryland Way*

3.5 The 2009 proposal extended out to a depth of 4.2m at single storey level, and a maximum of 4.2m at first floor level to the centre of the roof, with two smaller first floor elements at depths of 2m and 2.7m respectively.

3.6 The 2008 proposal (08/00806/HOU) was refused for the following reason:

*The proposed rear extension by reason of its overall scale and location would have an unacceptable overbearing relationship with the rear of the adjacent property no 34, Maryland Way and its rear garden*

3.7 The 2008 proposal extended out to a depth of 4.2m at single storey level, and a maximum of 4.2m at first floor level to the centre of the roof, with two smaller first floor elements at depths of 2m and 2.7m respectively.

3.8 Copies of the proposed plans and elevations are provided as an Appendix.

#### **4. Consultations**

4.1 The following table shows those bodies consulted and their response.

Consultee	Comment
County Highway Authority	No objection
Environmental Health	No comments

#### **5. Public Consultation**

5.1 9 letters of notification were sent out to neighbouring properties. At the time of writing 10 letters of representation had been received from 8 separate addresses. The following concerns have been raised:

- Loss of daylight
- Poor outlook from side window to bedroom
- Overshadowing from increased footprint
- Out of proportion compared to surrounding properties/overdevelopment
- Design shows insufficient regard to host property
- Visually Intrusive/overbearing/poor outlook
- Previous applications have been refused or withdrawn
- Loss of privacy
- Loss of sunlight
- Separation distance to side boundaries below prescribed 1m
- Special consideration should be given to the elderly resident at no. 34 Maryland Way
- Noise, dust and disturbance during construction
- Breach of the neighbours Human Right for the 'peaceful enjoyment of his home'

## **6. Planning Issues**

- Design and appearance
- Impact on neighbouring properties
- Other matters

## **7. Planning Considerations**

### Design and appearance

- 7.1 Policy EN1(a) of the Core Strategy & Policies DPD (CS & P DPD) states that the Council will require a high standard in the design and layout of new development. Proposals for new development should demonstrate that they will create buildings and places that are attractive with their own distinct identity; they should respect and make a positive contribution to the street scene and the character of the area in which they are situated, paying due regard to the scale, height, proportions, building lines, layout, materials and other characteristics of adjoining buildings and land.
- 7.2 The existing property is a chalet-style property that is modest in scale and similar in design to the other 'Berg' style properties in the area. A previous application (ref 15/00804/HOU) was withdrawn and this application has been submitted to try to overcome the previous concerns of the Council Officer. These included the bulk and mass of the proposal when viewed from the adjoining properties by virtue of the full height two storey extension and not paying due regard to the character of the area and the host building which has a chalet style design with accommodation within the first floor roof space with dormer windows.
- 7.3 The design of the rear extension has been amended from the previously withdrawn application so that the integrity of the existing main roof form is maintained and the rear extension maintains a 'chalet' style. It has also been designed so that the roof of the rear extension slopes away from the side boundaries at first floor level. This allows the building, when viewed from the neighbouring properties to the side and for those parts of the extension visible from the street scene to continue to provide a chalet style appearance with accommodation within the roofspace. As such the previous large flat roofed area of the previous proposal that resulted in a bulky addition, has been omitted. The current design with the dual pitch roof at the rear will mean that when the property is viewed from the gaps between the buildings from the street scene, the main roof form is retained. Similar designs have been accepted within the locality, including one recently at 55 Maryland Way, nearly opposite this site and also one nearby at 47 Queensway which was presented to this Committee in July of last year. The height of the main roof will remain as existing. A condition can ensure that it will be built of materials to match the existing dwelling. It is considered that the proposal respects the scale and design of the host building and conforms to policy EN1.

### Impact on adjoining properties

- 7.4 Policy EN1(b) of the Core Strategy & Policies DPD (CS & P DPD) states that the Council will require proposals for new development to demonstrate that

they will achieve a satisfactory relationship to adjoining properties avoiding significant harmful impact in terms of loss of privacy, daylight or sunlight, or overbearing effect due to bulk and proximity or outlook.

- 7.5 The proposed 2 storey rear extension does not cross the 45 degree horizontal or vertical line when taken from the windows of either of the neighbouring properties, as required by the Councils Supplementary Planning Document for the Design of Residential Extensions (SPD). In the case of no. 30, there is an existing rear extension which means that this proposal would not infringe the horizontal angle taken from the patio doors to the rear of this property. The sloping roof means that the vertical angle is also not infringed.
- 7.6 In the case of the other adjoin property at no.34, there is a glazed door on the rear elevation and a window serving a kitchen where both the 45 degree horizontal and vertical angle are not infringed. There is a rear extension to this property forming a lounge / sun room with a large expanse of glazing to the side. However, once again the 45 degree vertical angle from this window is not broken by the extension. Due to the design of the extension with the sloping roof it is not considered that the proposals would give rise to any overbearing or visual intrusion or sufficiently impact upon the visual outlook from adjoining properties and their gardens to justify refusal. The proposal does not result in any significant loss of daylight or sunlight to the rear of the adjoining properties.
- 7.7 The first floor side facing bedroom window at no. 34 adjoining is the sole source of light to that room. The window is relatively wide allowing significant amounts of light to enter and a wide angle of view to both the street and rear gardens. Due to the proposed angled design of the roof away from the adjoining properties, it is not considered that there will be a significant loss of outlook or light to this window. There is also a window and door at ground floor level on the side elevation. The door is obscurely glazed and both appear to serve a garage / store and already look directly onto the flank elevation of the application site. Therefore no adverse impacts are considered to affect these windows.
- 7.8 The three rear facing first floor windows of the extension at the application site serve a bedroom and en-suite. While one may be obscurely glazed, the other windows will look outwards to the rear of the property. However, the rear extension extends further outwards beyond the rear of the adjoining property at no. 30 with no scope to overlook the 3m area immediately to the rear of no. 30. There is also an existing dormer window to the rear now with potentially greater scope for overlooking than the property when extended. Therefore it is not considered that any significant loss of privacy will result. No new windows will be inserted into the side elevations of the proposal (with the exception of a bathroom rooflight which will be obscurely glazed) and a condition restricting any flank windows can be imposed.
- 7.9 The proposal maintains the same separation distances to the side boundaries that currently exist. In the case of the boundary to no. 34, at 90cm this is slightly below the Councils minimum 1m separation distance applying to 2 storey extensions as required by the SPD on design to ensure that the gaps between the dwellings are maintained and no terracing effect is created.



However as the roof slopes away from the boundary it may be considered as single storey rather than two storey at this point and at which the separation distance does not apply. The submitted plans show that the distance to the other boundary at no. 30 is 1.025m although a representation letter states this is also below 1m. However the same consideration would apply to this boundary as with that to no. 30.

- 7.10 The rear extension will be some 18m from the rear boundary which exceeds the minimum separation distance from back to boundary development of 10.5m as set out in the SPD.
- 7.11 Therefore the proposal will have an acceptable relationship with the neighbouring dwellings and will not have a significant impact on their amenity. Therefore the proposal conforms to policy EN1.

#### Other matters

- 7.12 Particular concerns have been raised by the elderly neighbour at no. 34 Maryland Way regarding the potential impacts upon himself and human rights points raised on his behalf. In view of the size of the plot, the design and size of the extension, it is not considered to represent a disproportionate addition or overdevelopment and no significant amenity impacts. There is only minimal demolition proposed and while there will inevitably be some noise and disturbance during any building works, it is not considered that this will be sufficient to justify refusal or unacceptably infringe anyone's 'human rights'.

#### Conclusion

- 7.13 It is considered that the design and style of the proposed extension is acceptable and pays sufficient regard to the host property and the character and appearance of the area. The proposal complies with the guidance contained in the Design SPD and after careful consideration, it is not considered that it would result in any significant adverse impacts upon the residential amenity of adjoining properties. Accordingly, the application is recommended for approval.

### **8. Recommendation**

- 8.1 GRANT subject to the following conditions:-

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:- This condition is required by Section 91 of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans and drawings:

MW/32/01 revision A received 08 October 2015.

Reason:- For the avoidance of doubt and in the interest of proper planning

3. The development hereby permitted must be carried out in facing materials to match those of the existing building in colour and texture.

Reason:- To ensure a satisfactory external appearance in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

4. That no further openings of any kind be formed in the side elevations of the extension hereby permitted without the prior written consent of the Local Planning Authority.

Reason:-To safeguard the amenity of neighbouring residential properties

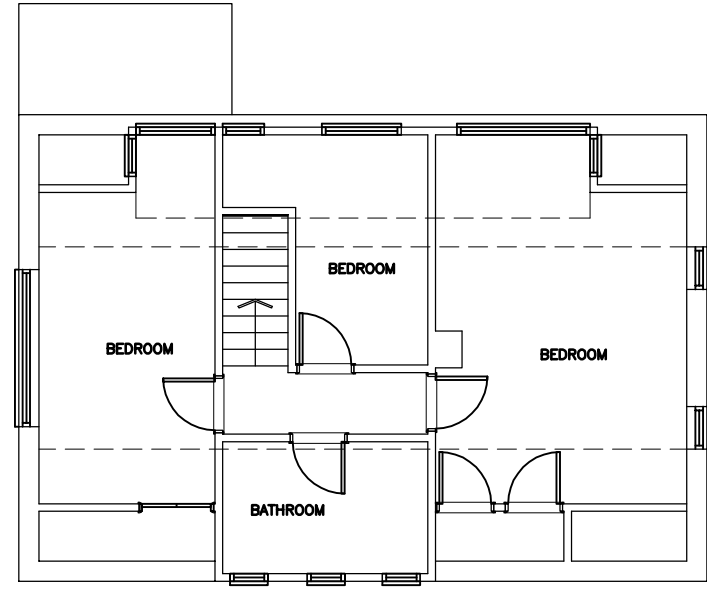
5. Prior to the occupation of the development hereby permitted the first floor bathroom rooflight window on the eastern side elevation shall be obscure glazed and be non-opening to a minimum height of 1.7 metres above internal floor level in accordance with details/samples of the type of glazing pattern to be submitted to and approved in writing by the Local Planning Authority. The window shall thereafter be permanently retained as installed.

Reason:- To safeguard the privacy of the adjoining properties, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

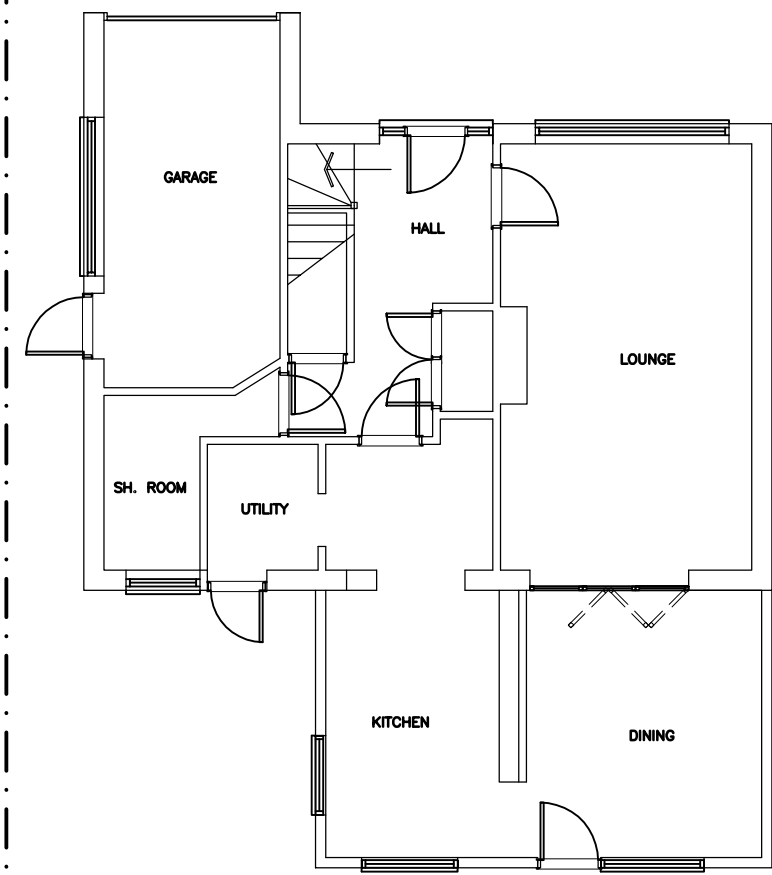
#### Informatives

1. In assessing this application, officers have worked with the applicant in a positive and proactive manner consistent with the requirements of paragraphs 186-187 of the NPPF. This included the following:-
  - a) Provided feedback through the validation process including information on the website, to correct identified problems to ensure that the application was correct and could be registered;
  - b) Have proactively communicated with the applicant through the process to advise progress, timescales or recommendation.
2. The applicant should be mindful to avoid significant noise and disturbance during the construction process and be mindful not create dust nuisance during building works.

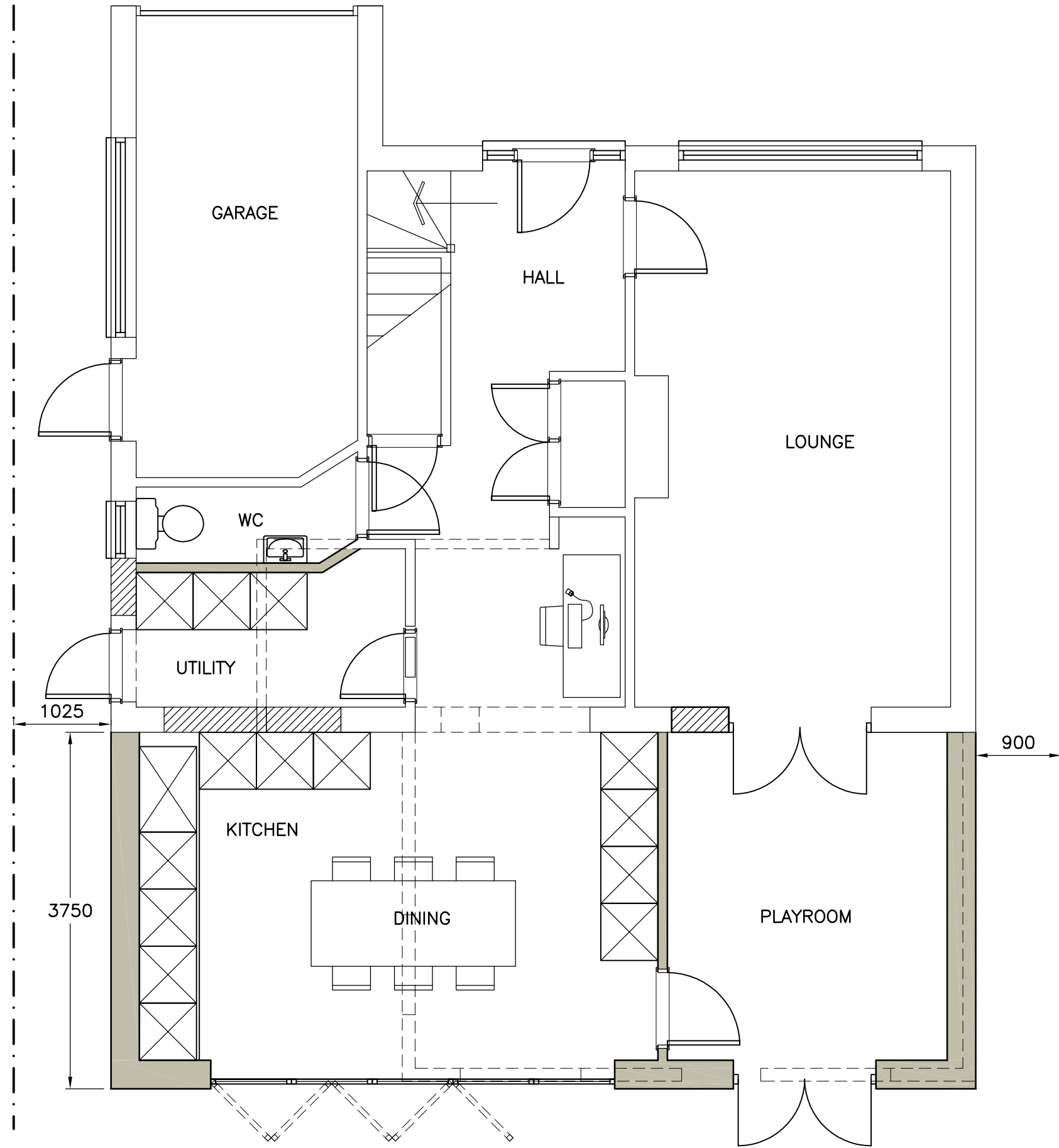




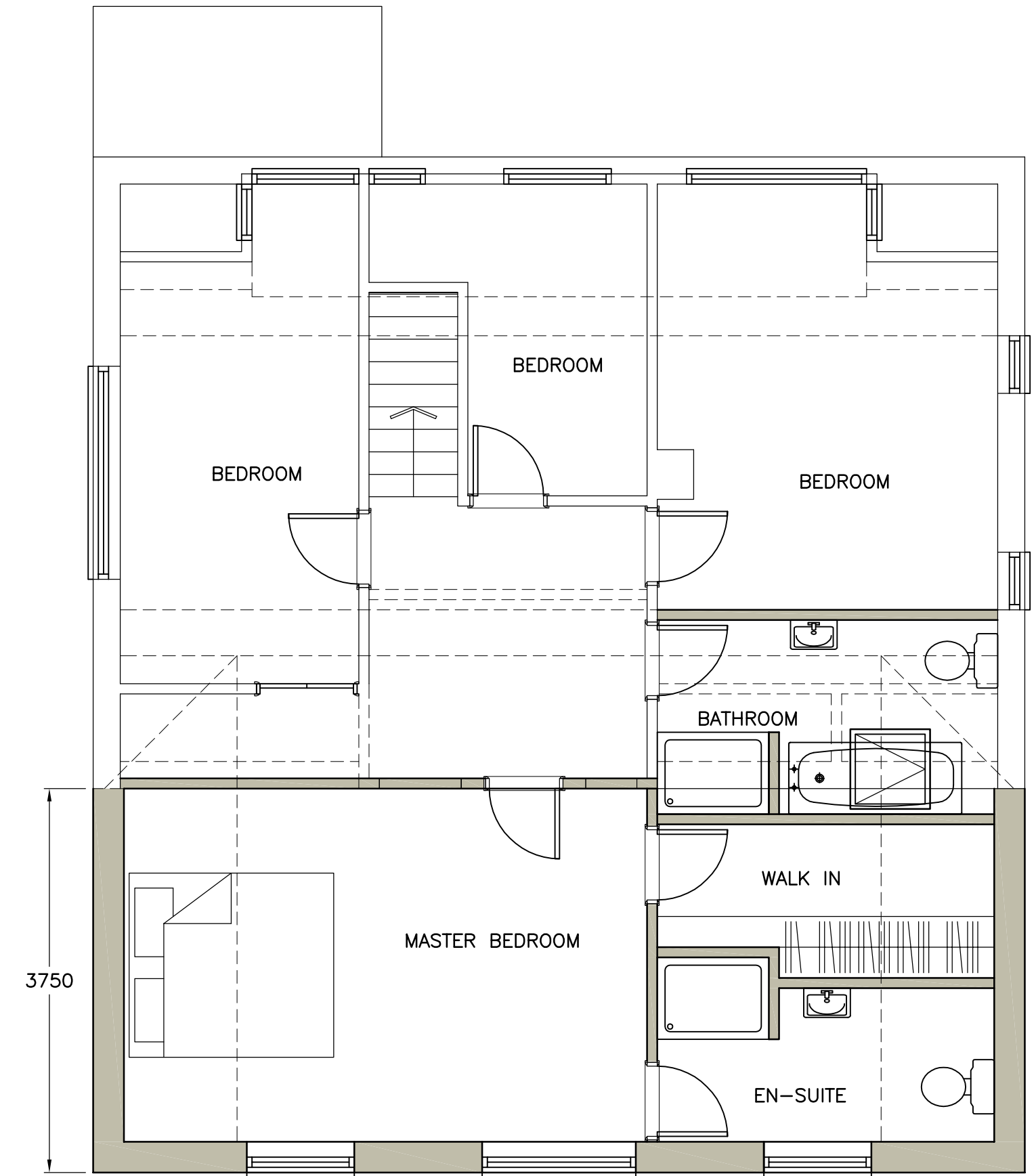
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EXISTING GROUND FLOOR



PROPOSED GROUND FLOOR



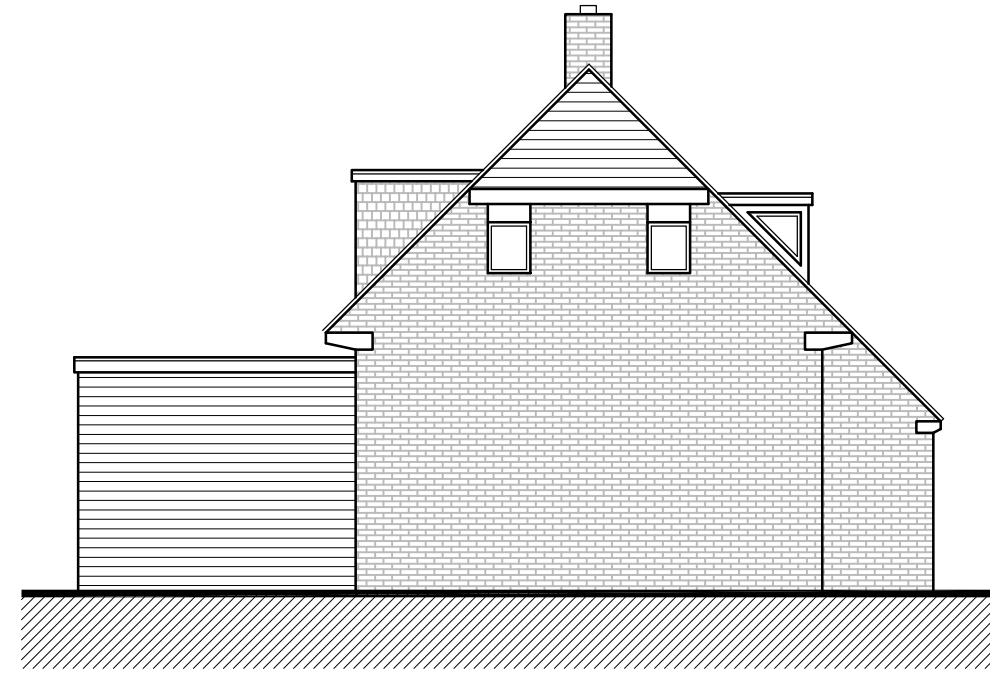
PROPOSED FIRST FLOOR



EXISTING REAR ELEVATION



EXISTING SIDE ELEVATION



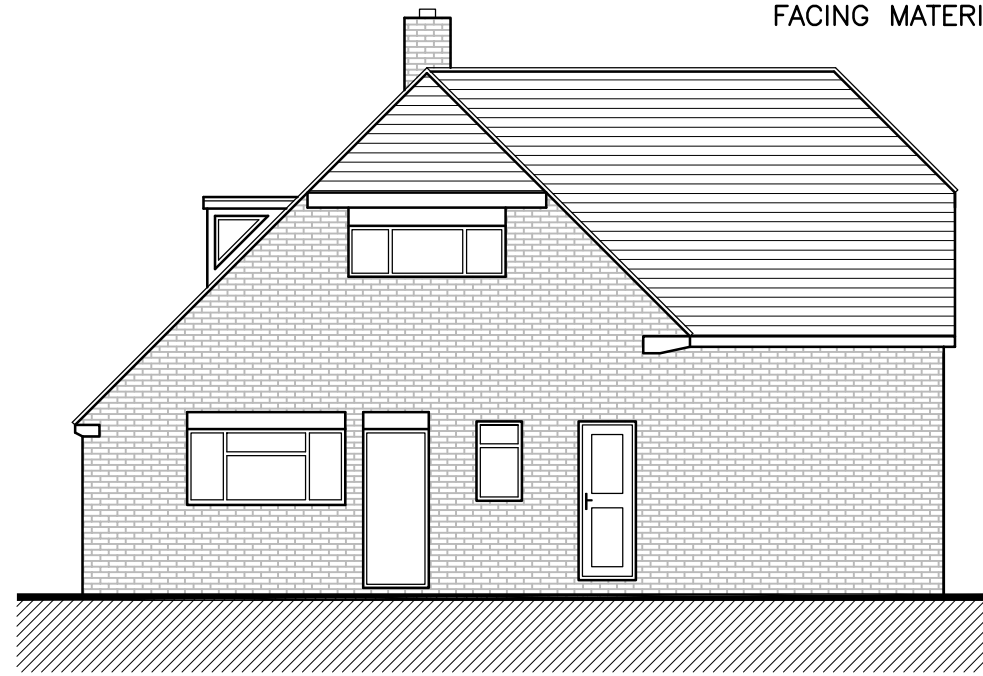
EXISTING SIDE ELEVATION



EXISTING FRONT ELEVATION

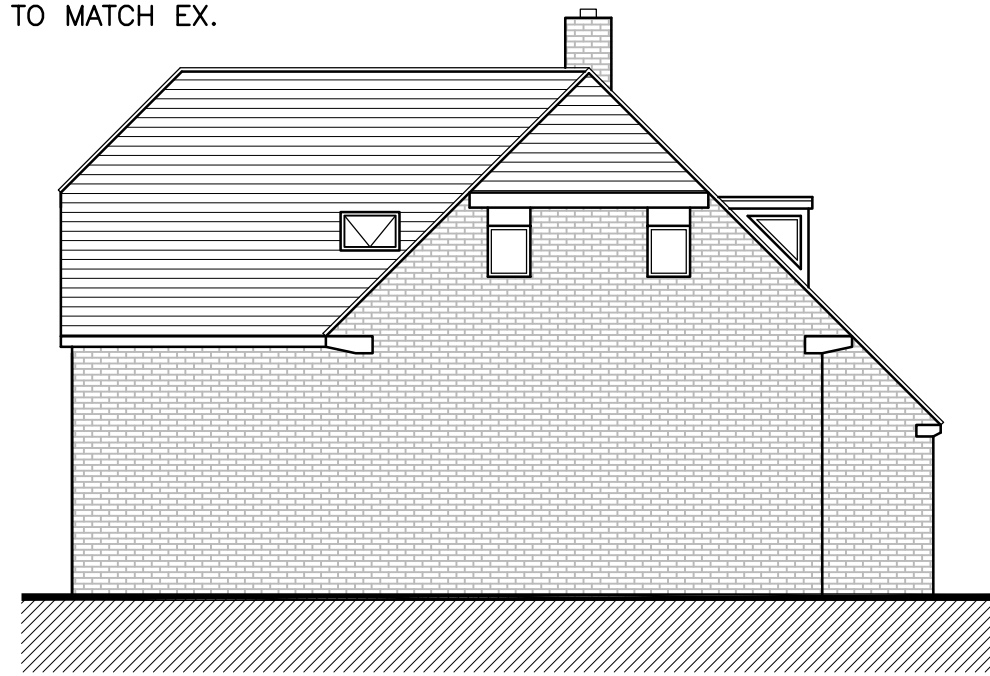


PROPOSED REAR ELEVATION



PROPOSED SIDE ELEVATION

FACING MATERIALS TO MATCH EX.



PROPOSED SIDE ELEVATION



PROPOSED FRONT ELEVATION



**PLANNING APPEALS**

**LIST OF APPEALS SUBMITTED BETWEEN 4 DECEMBER AND 17 DECEMBER 2015**

<b><u>Planning Application/ Enforcement No.</u></b>	<b><u>Inspectorate Ref.</u></b>	<b><u>Address</u></b>	<b><u>Description</u></b>	<b><u>Appeal Start Date</u></b>
15/00098/PLNCON	APP/Z3635/C/15/3136634	22 Thames Meadow, Shepperton	The unauthorised residential use of the boat, land and mooring and storage of vehicles, white goods, building materials and general debris on Green Belt land without planning permission.	03/12/2015
12/00246/ENF	APP/Z3635/C/15/3140328	48 Park Road, Ashford		15/12/2015

**APPEAL DECISIONS RECEIVED BETWEEN 4 DECEMBER AND 17 DECEMBER 2015**

<b>Site</b>	Land To The North of Chertsey Road, Shepperton
<b>Planning Application Number:</b>	14/01611/FUL
<b>Appeal Reference</b>	APP/Z3635/W/15/3051152
<b>Appeal Decision Date:</b>	23 November 2015
<b>Inspector's Decision</b>	The appeal is dismissed.
<b>Proposal</b>	Erection of stables with feed and tack store. Provision of new access to Chertsey Road (opposite Fairford) and associated roadway.
<b>Reasons for refusal</b>	The development represents inappropriate development in the Green Belt for which no very special circumstances have been

	demonstrated. It will result in the site having a more urban character, will diminish the openness of the Green Belt and conflict with the purposes of including land within it. It is therefore contrary to Policy GB1 of the Spelthorne Borough Local Plan 2001 and Section 9 (Protecting Green Belt Land) of the Government's National Planning Policy Framework 2012.
<b>Inspector's Comments</b>	<p>The Inspector considered that the proposed stable building comprising 8 stable units and a substantial food and tack store was excessive in size in relation to the size of the overall site. In particular, he concluded that the stables and food/tack store area were larger than reasonably required for the use, and that the facility did not constitute an "appropriate facility" within the terms of Green Belt policy in the National Planning Policy Framework. He also considered that the proposed access road in terms of its length and width would have a more developed and urbanised appearance to the site. Moreover, he stated that the more developed nature of the site would contrast with the more rural and open aspect of the land and would harm the character and appearance of the area.</p> <p>Consequently, the Inspector concluded that the proposal constituted 'inappropriate development' in the Green Belt, that it would harm the openness of and the conflict with the purposes of the Green Belt, and would harm the character of the area. He considered that no 'very special circumstances' existed that would outweigh the substantial harm to the Green Belt.</p>

<b>Site</b>	Splash Cottage, Parke Road, Sunbury on Thames
<b>Planning Application Number</b>	15/00217/HOU
<b>Appeal References</b>	APP/Z3635/D/15/3132875
<b>Appeal Decision Date:</b>	09/12/2015
<b>Inspector's Decision</b>	The appeal is dismissed.
<b>Proposal</b>	Erection of first floor extension, enclosure of existing ground floor balcony and creating larger basement by enclosing walled area.
<b>Reason for Refusal</b>	The proposed dwelling by reason of its scale, design and siting would have a poor relationship with neighbouring residential



	<p>properties resulting in a loss of light to the front facing window of No. 13 Fordbridge Road, loss of outlook to the side facing windows of No. 9 Fordbridge Road and causing a loss of light and being overbearing to both adjacent properties. As such the proposal will cause a significant loss of amenity to neighbouring residential properties contrary to Policy EN1 of the Core Strategy and Policies DPD 2009 and the Supplementary Planning Document on the Design of Residential Extensions and New Residential Development 201.</p> <p>The proposed dwelling by reason of its scale, design and siting would represent an overdevelopment of the site, resulting in development that is out of character and of detriment to the locality due to the reduction in gaps between buildings, and overall depth, width and height of the resultant property dominating the site. The proposal is not considered to make a positive contribution to the street scene and is contrary to Policy EN1 of the Core strategy and Policies DPD 2009.</p>
<p><b>Inspector's Comments</b></p>	<p>In terms of the decision, the Inspector has dismissed the appeal based on the impact on the amenity of no. 9 and the proposed dwelling being out of character.</p> <p>The Inspector noted that, <i>'Splash cottage relates much more to the properties to the north-east with their spacious character than those to the south-west. This is because the single storey nature of the building, along with its style and materials, create a sense of space around it rather than the more dense development to the southwest.'</i> He previously noted that the dwelling to the north east are either bungalows or two storey with the upper floor rooms being in the roof and with significant gaps between them.</p> <p>He referred to the proposal being 'deeper', having a 'greater volume, higher eaves line and lower pitched roof' and as a result <i>'...would appear more bulky in the street scene. It would also have a greater bulk than 9 Fordbridge Road as this property does not have the same apparent width when viewed from Fordbridge Road as the extension on its south western side is single storey with a flat roof thereby reducing the overall bulk.'</i></p> <p>He went on to note that, <i>'the proposal would fill most of the width of the site, creating a lack of space on either side, particularly above first floor level, which is characteristic of those properties to the north-east which, as I have found the site more closely relates. It would therefore be harmful to the overall character and appearance of the area and the street scene in particular.'</i> He noted that it did not respond to local character as required by the National Planning Policy Framework.</p> <p>In relation to the impact on no. 9 he stated that , <i>'...The full two storey height very close to the boundary would result in an</i></p>

	<p><i>overbearing effect in close proximity to this property and its patio, and would result in a loss of outlook from No. 9. The proposal would therefore have a significant harmful effect on the living conditions which the occupiers might reasonably expect to enjoy.'</i></p> <p>He sympathised with the appellant's personal circumstances, and noted that the adverse effect of the development would be permanent whereas such personal circumstances will change over time and as such does not outweigh the serious harm he found.</p> <p>He concluded that it would fail to comply with relevant local plan policies and the guidance in the Framework.</p>
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**FUTURE HEARING / INQUIRY DATES**

<b>Council Ref.</b>	<b>Type of Appeal</b>	<b>Site</b>	<b>Proposal</b>	<b>Case Officer</b>	<b>Date</b>
15/00087 /ENF	Hearing	The Willows, Moor Lane, Staines Upon Thames.	Enforcement notice relating to the unauthorised storage on open land.	JF	15/03/2016